



STAFF REPORT

Report To: Board of Supervisors

Meeting Date: March 16, 2017

Staff Contact: Hope Sullivan, Planning Manager

Agenda Title: For Possible Action: To consider a request from Blackstone Development Group Inc. for a Tentative Subdivision Map known as Blackstone Ranch Phase 1, consisting of 189 single family residential lots on property approved for Single Family 6000 zoning, located south of East Robinson Street, east of North Saliman Road, and north of East Fifth Street, APN 010-041-70. (Hope Sullivan, hsullivan@carson.org)

Staff Summary: Per the provisions of Section 17.05.010 of the Carson City Municipal Code (CCMC), the Board of Supervisors is authorized to approve a Tentative Subdivision Map. The Planning Commission makes a recommendation to the Board. The Planning Commission reviewed the subject application at its meetings of February 22, 2017, and has recommended approval of the request based on the ability to make the required findings subject to Conditions of Approval.

Agenda Action: Formal Action/Motion

Time Requested: 30 minutes

Proposed Motion

I move to approve TSM-17-005, a Tentative Subdivision Map known as Blackstone Ranch Phase 1, consisting of 189 single family residential lots on property approved for Single Family 6000 zoning, located south of East Robinson Street, east of North Saliman Road, and north of East Fifth Street, APN 010-041-70, based on the ability to make the required findings in the affirmative and subject to the conditions of approval contained in the Memorandum dated March 3, 2017 from the Planning Manager to the Board of Supervisors.

Board's Strategic Goal

Quality of Life

Previous Action

At its meeting of February 22, 2017, the Planning Commission voted 5-0, 2 absent to recommend approval of the subject application based on the ability to make the required findings in the affirmative subject to conditions of approval. The conditions of approval recommended by the Planning Commission are included in the Memorandum dated March 3, 2017 from the Planning Manager to the Board of Supervisors.

Background/Issues & Analysis

Please see attached staff report to the Planning Commission with attachments.

Applicable Statute, Code, Policy, Rule or Regulation

CCMC 17.05 (Tentative Maps), CCMC 17.07.005 (Findings); Lompa Ranch Specific Plan

Financial Information

Is there a fiscal impact? Yes No

If yes, account name/number:

Is it currently budgeted? Yes No

Explanation of Fiscal Impact:

Alternatives

1. Approve the Tentative Subdivision Map subject to conditions different than those recommended by the Planning Commission based on the ability to make the required findings.
2. Deny the Tentative Subdivision Map based on the inability to make the required findings, stating which findings can not be made.
3. If additional information is submitted to the Board of Supervisors that the Board believes warrants further review and consideration of the application by the Planning Commission, refer the matter back to the Planning Commission.

Board Action Taken:

Motion: _____

1) _____

2) _____

Aye/Nay

(Vote Recorded By)



Carson City Planning Division

108 E. Proctor Street
Carson City, Nevada 89701
(775) 887-2180 – Hearing Impaired: 711
planning@carson.org
www.carson.org/planning

MEMORANDUM

Board of Supervisors Meeting of March 16, 2017

TO: Board of Supervisors

FROM: Hope Sullivan,
AICP Planning
Manager

DATE: March 3, 2107

SUBJECT: **TSM-17-005 – A request from Blackstone Development Group Inc. for a Tentative Subdivision Map approval to create 189 single-family residential lots on 44.5 acres on south of East Robinson Street, east of North Saliman Road, and north of East Fifth Street on land approved for Single Family 6000 zoning. APN 010-041-70.**

DISCUSSION

At its meeting of February 22, 2017, the Planning Commission recommended approval of the request subject to the following conditions of approval. The Planning Commission recommendation differs from the staff recommendation, specifically with respect to Conditions 40 and 57. Planning Commission recommended language being deleted is indicated with a strike-through. Planning Commission added language is underlined.

RECOMMENDED CONDITIONS OF APPROVAL

The following are general conditions of approval:

The following are conditions of approval required per CCMC 18.02.105.5:

1. All final maps shall be in substantial accord with the approved tentative map.
2. Prior to submittal of any final map, the Development Engineering Department shall approve all on-site and off-site improvements. The applicant shall provide construction plans to the Development Engineering Department for all required on-site and off-site improvements, prior to any submittals for approval of a final map. The plan must adhere to the recommendations contained in the project soils and geotechnical report.
3. Lots not planned for immediate development shall be left undisturbed and mass grading and clearing of natural vegetation shall not be allowed. Any and all grading shall comply with City standards. A grading permit from the Nevada Division of Environmental

Protection shall be obtained prior to any grading. Noncompliance with this provision shall cause a cease and desist order to halt all grading work.

4. All lot areas and lot widths shall meet the zoning requirements approved as part of this tentative map with the submittal of any final map.
5. With the submittal of any final maps, the applicant shall provide evidence to the Planning and Community Development Department from the Health and Fire Departments indicating the agencies' concerns or requirements have been satisfied. Said correspondence shall be included in the submittal package for any final maps and shall include approval by the Fire Department of all hydrant locations.
6. The following note shall be placed on all final maps stating:

"These parcels are subject to Carson City's Growth Management Ordinance and all property owners shall comply with provisions of said ordinance."

7. All other departments' conditions of approval, which are attached, shall be incorporated as conditions of this report.
8. Placement of all utilities, including AT&T Cablevision, shall be underground within the subdivision. Any existing overhead facilities shall be relocated prior to the submittal of final maps.
9. The applicant must sign and return the Notice of Decision for conditions for approval within ten (10) days of receipt of notification after the Board of Supervisors meeting. If the Notice of Decision is not signed and returned within ten (10) days, then the item may be rescheduled for the next Planning Commission meeting for further consideration.
10. Hours of construction will be limited to 7:00 a.m. to 7:00 p.m., Monday through Friday, and 7:00 a.m. to 5:00 p.m. on Saturday and Sunday. If the hours of construction are not adhered to, the Carson City Building Department will issue a warning for the first violation, and upon a second violation, will have the ability to cause work at the site to cease immediately.
11. The applicant shall adhere to all City standards and requirements for water and sewer systems, grading and drainage, and street improvements.
12. The applicant shall obtain a dust control permit from the Nevada Division of Environmental Protection. The site grading must incorporate proper dust control and erosion control measures.
13. A detailed storm drainage analysis, water system analysis, and sewer system analysis shall be submitted to the Development Engineering Department prior to approval of a final map.
14. Prior to the recordation of the final map for any phase of the project, the improvements associated with the project must either be constructed and approved by Carson City, or the specific performance of said work secured, by providing the City with a proper surety in the amount of one hundred fifty percent (150%) of the engineer's estimate. In either case, upon acceptance of the improvements by the City, the developer shall provide the City with a proper surety in the amount of ten percent (10%) of the engineer's estimate to secure the developer's obligation to repair defects in workmanship and materials which appear in the

work within one (1) year of acceptance by the City. Improvements associated with the Conditional Letter of Map Revision must be constructed and may not be secured for in lieu of construction.

15. A "will serve" letter from the water and wastewater utilities shall be provided to the Nevada Health Division prior to approval of a final map.
16. The District Attorney shall approve any CC&R's prior to recordation of the first final map.

Specific Conditions to be included in the Design of the Improvement Plans, to be met prior to approval of construction permit:

17. The improvement plans shall include all improvements identified in the approved Phasing Plan as being executed as part of Phase A1.
18. In order to allow for the full functionality of East Robinson Street, the applicant shall work with the School District to move the loading and unloading school bus function off of East Robinson Street. The identified solution shall be included in the improvement plans.
19. Project must comply with the currently adopted fire code and applicable amendments adopted by Carson City.
20. Hydrant spacing as shown on Alexis Ave doesn't meet the 2012 IFC Appendix C spacing.
21. The cul-de-sac at the end of Achilles Street is too small. It must comply with the 2012 IFC Appendix D figure D103.1
22. The traffic must have at least 20' total width for travel lanes.
23. The improvement plans must demonstrate compliance with CCMC Title 18 Division 15.5, and all applicable codes found in Chapters 7 and 10 of the 2012 Uniform Plumbing Code.
24. Two parking spaces must be provided per residence per Division 2.2 of the Carson City Development Standards. Parking spaces must not be tandem. If the driveway is used to count towards parking spaces, the driveway must be the appropriate length and width for the number of spaces per standard detail C-5.5.1
25. At the end of the cul-de-sac, at the southeast end of the development, additional drainage/access space must be provided to give City maintenance equipment sufficient space to turn around.
26. Underground storm drain systems that connect to flood conveyance channels shall do so completely above the base flood elevation of the channel.
27. Any changes to the Master Phasing Drainage Study must be reflected in changes to the drainage study for the subject subdivision.
23. Low Impact Design (LID) measures will be required to be implemented as part of the development storm drainage system.
24. The Conditional Letter of Map Revision (CLOMR) must be approved by FEMA prior to approval of any construction permits. All improvements associated with the CLOMR must be included in the improvement plans.

25. The CC&R's must clearly state that a Landscape Maintenance District (LMD), a Home Owners Association (HOA) or similar entity is responsible for maintaining private storm drain infrastructure including any mains, basins, and LID infrastructure.
26. The minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
27. The irrigation diversion structure on the north side of E 5th St must be shown in the improvement plans and referenced in the technical drainage study for the subdivision.
28. The flood conveyance channels necessary for the CLOMR must be built with this subdivision. A sedimentation basin must be constructed as part of these improvements at the 90 degree turn of the Vicee channel.
29. Alexis Avenue must be renamed to make a continuation of Appaloosa.
30. Landscaping plans for the construction permit must include site distance triangles showing that sight distance is not inhibited.
31. Landscaping plans for the construction permit must show distances to existing and proposed water, sewer and storm drain mains to ensure a minimum of 10 foot spacing from trees.
32. A 10 foot wide multiuse pedestrian access path must be provided between the development and the future phase to the south.
33. The following street names cannot be used: Adam St, Maximus Ave, Alexis Ave, and Dianna St.
34. The utility plans for the construction permit must indicate precast manholes and bases. Cast in place manholes will not be allowed, regardless depth of new sewer mains. Riser depths must meet Carson City Standard Details.
35. A geotechnical report will be required for the subdivision prior to approval of any construction permits.
36. Local roads will have a minimum ACC pavement thickness of 4 inches.

Conditions to be Addressed with the Final Map

37. A development agreement reflecting the approved phasing plan for the Lompa Ranch Specific Plan must be fully executed prior to Final Map approval. The development agreement will address drainage, water, sewer, roadways and traffic, parks, recreation, trails, open space, and fire station improvements, including the timing of improvements, design standards, funding, and operation and maintenance responsibilities consistent with the conditions of approval with this Tentative Map and the Lompa Ranch Specific Plan.
38. All channels and associated access must be shown as separate parcels on the final map to be dedicated to the City.
39. The final mylar will be present to the State Engineer for approval and signature.

Conditions Related to the Lompa Ranch Specific Plan Phasing Plan

40. The following change must be made to the master phasing plans and reports. The master phasing plans and reports must be included as part of a development agreement for the entire SPA area prior to approval of any construction permit ~~or any tentative map acceptance, whichever comes first.~~
41. Note that geotechnical investigations will be required to establish design parameters for individual phases.

WATER:

42. The parallel water mains in the phasing plan must be shown as one single water main, however looping must be maintained.
43. The water infrastructure improvements exhibit must be updated to accurately reflect the new facilities required or predecessor to each phase. This column must indicate which mains are required to create looping for each phase. For instance, phase B1 calls for a main to be extended along Robinson Street, however a main would also need to be extended along the Spine Road from 5th Street in order to create a loop.
44. Note that no water main shall have more than 15 services without looping.

SEWER:

45. The sewer infrastructure improvements exhibit must note that cast in place manholes will not be allowed, regardless depth of new sewer mains, and that riser depths must meet Carson City Standard Details.

STORM DRAIN & FLOOD MANAGEMENT:

46. Plans must show and note that all flood channels (Vicee Canyon, Ash Canyon, and Kings Canyon) must provide sufficient access for City maintenance equipment along the full length, with access points spaced out no more than every 660 feet, and must note that Robinson St, the Spine Road, E 5th St, and N Saliman Rd are not to be considered part of this access.
47. Note that all flood channels and associated access must be on separate parcels to be dedicated to the City. Maintenance of these lands will be funded through an maintenance district or similar instrument, to be established prior to Final Map approval.
48. Note that privately owned and maintained LID/Water Quality facilities are required for each development.
49. The phasing drainage study must demonstrate the ability of downstream drainage facilities to handle increased runoff if detention is not used.
50. Note in the phasing plan that all structures must meet the Flood Protection Ordinance where the lowest floor is two feet above the base flood elevation of the FEMA 1% chance flood or the onsite 1% chance flood whichever is higher.
51. Note in the phasing plan that the minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
52. Note in the phasing plan that any crossings of flood channels must meet a 100-year flow capacity plus 18 inches of freeboard and must be a clear opening, no multi barrel pipes.

53. Note that drainage studies for all development phases shall demonstrate compliance with Floodplain Storage Capacity Protection requirements of CCMC 12.09.080 (9).
54. Note that drainage studies for all development phases shall provide emergency flow paths for a one hundred (100) year peak storm in accordance with Development Standards.
55. The developer must design the Kings Canyon flood channel such that the drainage and/or any water rights associated with parcels 010-041-34 and 010-041-035 are not adversely affected.

TRAFFIC:

56. Note that Traffic Impact Studies required for all phases must demonstrate that the segment of N Saliman Rd between E William St and E Robinson St will have a projected level of service of C or better for year 2025 unless the North-South Spine Road is connected to William Street. The North-South Spine Road must connect to William Street prior to any development that would cause a level of service worse than C for this segment of road.
57. Note that Traffic Impact Studies required for all phases must demonstrate that the northbound leg and the westbound left turning movement of the N Saliman Rd/E William St intersection and the overall intersection will have a projected level of service of D or better for year 2025 unless the North-South Spine Road is connected to William Street. The North-South Spine Road must connect to E William Street prior to any development that would cause a level of service worse than D for the northbound leg or the westbound left turning movement of this intersection during school hours. Also note that traffic impact studies for each phase west of I580 will require traffic counts at this intersection.
58. Phasing plan maps must be updated to show east-west connectivity between streets in phase A1 and phase B1, and a 10 foot multiuse path between phase A1 and phase A2.

PARKS, TRAILS, AND OPEN SPACE

59. General Comments
 - a. The applicant will enter into a developer agreement with the City. This agreement will include terms and conditions for the funding of the design, construction, and dedication of park, recreation and path facilities within the Lompa Ranch North Specific Plan area. The agreement will outline the City's process for the collection and distribution of Residential Construction Tax (RCT) compliant with CCMC 15.60. The agreement must be considered and approved by the Board of Supervisors prior to recording the Final Map.
 - b. The phasing plan will be modified to identify phase triggers and unit counts for park, recreation and path facilities for the Lompa Ranch North Specific Plan area. The phasing plan will be modified and approved by the Board of Supervisors prior to recording the Final Map.
 - c. A private Home Owner's Association (HOA), Landscape Maintenance District (LMD), or similar instrument will be established for the Lompa Ranch North Specific Plan area to provide for the operations and maintenance of all park, recreation and path facilities. Operation and maintenance standards for these facilities will be established by the City. The applicant will draft an agreement for the Board of Supervisor's consideration and approval no later than issuance of the certificate of occupancy for the 200th residential unit.
 - d. A private Home Owner's Association (HOA), Landscape Maintenance Association (LMA), or similar instrument will be formed to provide 100% funding and maintenance for all the

following areas in perpetuity: Common landscape and open space areas, buffer areas between the development and neighborhoods, landscaping associated with the development's path system, landscape medians, street corridors, non-public recreation facilities/amenities, detention basins, and drainage channels. The maintenance and funding shall be addressed in the developer agreement to the satisfaction of the Board of Supervisors. Common area maintenance shall include at a minimum, but not limited to the following:

- i. Debris, weed, and litter removal
 - ii. Noxious and invasive weed management, including fire prevention
 - iii. Care and replacement of plant material
 - iv. Plant material irrigation and irrigation system repair
- e. As the development's phases are implemented, the plans will be submitted for review by the City. The applicant shall be required to demonstrate pedestrian connectivity between the neighborhood parks, "off-street/paved/shared" multi-use paths, and sidewalks. This shall be done to the satisfaction of the Parks, Recreation, and Open Space Department.
- f. All "off-street/paved/shared" multi-use paths and sidewalks will conform to the standards and policies outlined in of the Carson City Unified Pathways Master Plan adopted April 6, 2006 (as revised March 15, 2007) and as amended in the future. There will be adequate pedestrian connectivity, throughout the development that provides convenient and logical access to neighborhood parks and paths and enhances the overall sidewalk network within the development.
60. Neighborhood Park Comments (10 acre park / west of I-580)
- a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 400th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation and Open Space Department.
 - b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including water conservation design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.
 - c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 750th residential unit. Upon successful completion, final project acceptance of said work will be done to the satisfaction of the City, through its Parks, Recreation and Open Space Department.
61. Neighborhood Park Comments (3 acre park / east of I-580)
- a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 100th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation, and Open Space Department.
 - b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including water conservation

design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.

- c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 250th residential unit. Upon successful completion, final project acceptance of said work will be done to the satisfaction of the City, through its Parks, Recreation, and Open Space Department.

62. Off-street/paved/shared Path Comments

1. Robinson Street and the Spine Road shall be constructed as full street improvements, to City standards and engineering requirements, including the construction of "on-street bike lanes" and concrete "off- street/shared/paved" multi-use paths. The path along Robinson Street will be constructed on the road's south side and the path along the Spine Road will be constructed on the road's east side.
2. All multi-use paths will be designed and constructed to a 10' wide (minimum) AASHTO standard concrete multi-use path (off street/paved/shared) with an adjacent 3' wide decomposed granite path.
3. The multi-use paths will include landscaping with a variety of trees (either evergreen or deciduous) that will be planted at a rate of 1 tree per 50 lineal feet (tree groupings are acceptable) with a minimum of 4 shrubs per tree.
4. Path amenities include but are not limited to park benches/seating areas (per 1000 lineal feet of trail along the path), pet waste stations/trash cans, signage depicting direction and trail distance.

Other comments

63. The applicant is required to use best management practices during construction to prevent the spread of noxious and invasive weeds and will incorporate language in construction documents to ensure contractors and subcontractors comply. The Parks, Recreation and Open Space Department will assist the applicant with this condition.

STAFF REPORT FOR THE PLANNING COMMISSION MEETING OF FEBRUARY 22, 2016

FILE NO: TSM-17-005

AGENDA ITEM: F- 4

STAFF AUTHOR: Hope Sullivan, AICP
Planning Manager

TSM-17-005 – For Possible Action: To make a recommendation to the Board of Supervisors regarding a Tentative Subdivision Map application from Blackstone Development Group Inc. (property owner: Tom and Martha Keating Family Trust) to create a 189-unit single family residential subdivision on approximately 44.5 acres within the Lompa Ranch North Specific Plan Area on property approved for Single Family 6000 (SF6) zoning, located south of East Robinson Street, east of North Saliman Road, and north of E. Fifth Street, APN 010-041-70. (Hope Sullivan)

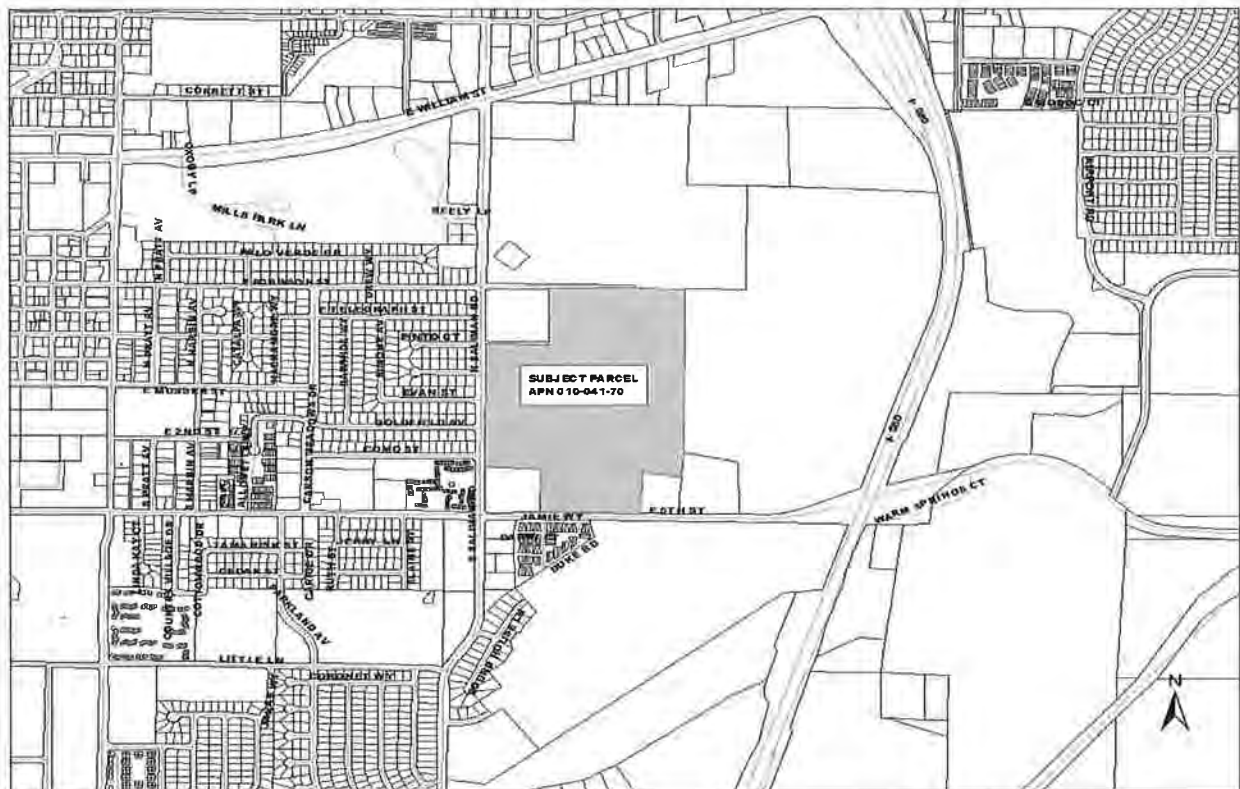
APPLICANT: Blackstone Development Group

OWNER: Thomas and Martha Keating Family Trust

LOCATION: south of East Robinson Street, east of North Saliman Road, and north of E. Fifth Street

APN: 010-041-70

SITE MAP



RECOMMENDED MOTION:

"I move to recommend to the Board of Supervisors approval of TSM-17-005, a Tentative Subdivision Map known as Blackstone Ranch, Phase 1, consisting of 189 single family residential lots on property approved for Single Family 6000 square feet (SF6), located south of East Robinson Street, east of South Saliman Road, and north of east Fifth Street APN 010-041-70, based on the findings and subject to the conditions of approval contained in the staff report."

RECOMMENDED CONDITIONS OF APPROVAL

The following are general conditions of approval:

The following are conditions of approval required per CCMC 18.02.105.5:

1. All final maps shall be in substantial accord with the approved tentative map.
2. Prior to submittal of any final map, the Development Engineering Department shall approve all on-site and off-site improvements. The applicant shall provide construction plans to the Development Engineering Department for all required on-site and off-site improvements, prior to any submittals for approval of a final map. The plan must adhere to the recommendations contained in the project soils and geotechnical report.
3. Lots not planned for immediate development shall be left undisturbed and mass grading and clearing of natural vegetation shall not be allowed. Any and all grading shall comply with City standards. A grading permit from the Nevada Division of Environmental Protection shall be obtained prior to any grading. Noncompliance with this provision shall cause a cease and desist order to halt all grading work.
4. All lot areas and lot widths shall meet the zoning requirements approved as part of this tentative map with the submittal of any parcel map or preferably final map.
5. With the submittal of any final maps, the applicant shall provide evidence to the Planning and Community Development Department from the Health and Fire Departments indicating the agencies' concerns or requirements have been satisfied. Said correspondence shall be included in the submittal package for any parcel map or preferably final maps and shall include approval by the Fire Department of all hydrant locations.
6. The following note shall be placed on all final maps stating:

"These parcels are subject to Carson City's Growth Management Ordinance and all property owners shall comply with provisions of said ordinance."
7. All other departments' conditions of approval, which are attached, shall be incorporated as conditions of this report.
8. Placement of all utilities, including AT&T Cablevision, shall be underground within the subdivision. Any existing overhead facilities shall be relocated prior to the submittal of a parcel map or preferably final maps.

9. The applicant must sign and return the Notice of Decision for conditions for approval within ten (10) days of receipt of notification after the Board of Supervisors meeting. If the Notice of Decision is not signed and returned within ten (10) days, then the item may be rescheduled for the next Planning Commission meeting for further consideration.
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11. The applicant shall adhere to all City standards and requirements for water and sewer systems, grading and drainage, and street improvements.
12. The applicant shall obtain a dust control permit from the Nevada Division of Environmental Protection. The site grading must incorporate proper dust control and erosion control measures.
13. A detailed storm drainage analysis, water system analysis, and sewer system analysis shall be submitted to the Development Engineering Department prior to approval of a final map.
14. Prior to the recordation of the final map for any phase of the project, the improvements associated with the project must either be constructed and approved by Carson City, or the specific performance of said work secured, by providing the City with a proper surety in the amount of one hundred fifty percent (150%) of the engineer's estimate. In either case, upon acceptance of the improvements by the City, the developer shall provide the City with a proper surety in the amount of ten percent (10%) of the engineer's estimate to secure the developer's obligation to repair defects in workmanship and materials which appear in the work within one (1) year of acceptance by the City. Improvements associated with the Conditional Letter of Map Revision must be constructed and may not be secured for in lieu of construction.
15. A "will serve" letter from the water and wastewater utilities shall be provided to the Nevada Health Division prior to approval of a final map.
16. The District Attorney shall approve any CC&R's prior to recordation of the first final map.

Specific Conditions to be included in the Design of the Improvement Plans, to be met prior to approval of construction permit:

17. The improvement plans shall include all improvements identified in the approved Phasing Plan as being executed as part of Phase A1.
18. In order to allow for the full functionality of East Robinson Street, the applicant shall work with the School District to move the loading and unloading school bus function off of East Robinson Street. The identified solution shall be included in the improvement plans.
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25. The CC&R's must clearly state that a Landscape Maintenance District (LMD), a Home Owners Association (HOA) or similar entity is responsible for maintaining private storm drain infrastructure including any mains, basins, and LID infrastructure.
26. The minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
27. The irrigation diversion structure on the north side of E 5th St must be shown in the improvement plans and referenced in the technical drainage study for the subdivision.
28. The flood conveyance channels necessary for the CLOMR must be built with this subdivision. A sedimentation basin must be constructed as part of these improvements at the 90 degree turn of the Vicee channel.
29. Alexis Avenue must be renamed to make a continuation of Appaloosa.
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Conditions to be Addressed with the Final Map

37. A development agreement reflecting the approved phasing plan for the Lompa Ranch Specific Plan must be fully executed prior to Final Map approval. The development agreement will address drainage, water, sewer, roadways and traffic, parks, recreation, trails, open space, and fire station improvements, including the timing of improvements, design standards, funding, and operation and maintenance responsibilities consistent with the conditions of approval with this Tentative Map and the Lompa Ranch Specific Plan.
38. All channels and associated access must be shown as separate parcels on the final map to be dedicated to the City.
39. The final mylar will be present to the State Engineer for approval and signature.

Conditions Related to the Lompa Ranch Specific Plan Phasing Plan

40. The following change must be made to the master phasing plans and reports. The master phasing plans and reports must be included as part of a development agreement for the entire SPA area prior to approval of any construction permit or any tentative map acceptance, whichever comes first.
41. Note that geotechnical investigations will be required to establish design parameters for individual phases.

WATER:

42. The parallel water mains in the phasing plan must be shown as one single water main, however looping must be maintained.
43. The water infrastructure improvements exhibit must be updated to accurately reflect the new facilities required or predecessor to each phase. This column must indicate which mains are required to create looping for each phase. For instance, phase B1 calls for a

main to be extended along Robinson Street, however a main would also need to be extended along the Spine Road from 5th Street in order to create a loop.

44. Note that no water main shall have more than 15 services without looping.

SEWER:

45. The sewer infrastructure improvements exhibit must note that cast in place manholes will not be allowed, regardless depth of new sewer mains, and that riser depths must meet Carson City Standard Details.

STORM DRAIN & FLOOD MANAGEMENT:

46. Plans must show and note that all flood channels (Vicee Canyon, Ash Canyon, and Kings Canyon) must provide sufficient access for City maintenance equipment along the full length, with access points spaced out no more than every 660 feet, and must note that Robinson St, the Spine Road, E 5th St, and N Saliman Rd are not to be considered part of this access.
47. Note that all flood channels and associated access must be on separate parcels to be dedicated to the City. Maintenance of these lands will be funded through an maintenance district or similar instrument, to be established prior to Final Map approval.
48. Note that privately owned and maintained LID/Water Quality facilities are required for each development.
49. The phasing drainage study must demonstrate the ability of downstream drainage facilities to handle increased runoff if detention is not used.
50. Note in the phasing plan that all structures must meet the Flood Protection Ordinance where the lowest floor is two feet above the base flood elevation of the FEMA 1% chance flood or the onsite 1% chance flood whichever is higher.
51. Note in the phasing plan that the minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
52. Note in the phasing plan that any crossings of flood channels must meet a 100-year flow capacity plus 18 inches of freeboard and must be a clear opening, no multi barrel pipes.
53. Note that drainage studies for all development phases shall demonstrate compliance with Floodplain Storage Capacity Protection requirements of CCMC 12.09.080 (9).
54. Note that drainage studies for all development phases shall provide emergency flow paths for a one hundred (100) year peak storm in accordance with Development Standards.
55. The developer must design the Kings Canyon flood channel such that the drainage and/or any water rights associated with parcels 010-041-34 and 010-041-035 are not adversely affected.

TRAFFIC:

56. Note that Traffic Impact Studies required for all phases must demonstrate that the segment of N Saliman Rd between E William St and E Robinson St will have a projected level of service of C or better for year 2025 unless the North-South Spine Road is

connected to William Street. The North-South Spine Road must connect to William Street prior to any development that would cause a level of service worse than C for this segment of road.

57. Note that Traffic Impact Studies required for all phases must demonstrate that the northbound leg and the westbound left turning movement of the N Saliman Rd/E William St intersection and the overall intersection will have a projected level of service of D or better for year 2025 unless the North-South Spine Road is connected to William Street. The North-South Spine Road must connect to E William Street prior to any development that would cause a level of service worse than D for the northbound leg or the westbound left turning movement of this intersection. Also note that traffic impact studies for each phase west of I580 will require traffic counts at this intersection.
58. Phasing plan maps must be updated to show east-west connectivity between streets in phase A1 and phase B1, and a 10 foot multiuse path between phase A1 and phase A2.

PARKS, TRAILS, AND OPEN SPACE

59. General Comments
 - a. The applicant will enter into a developer agreement with the City. This agreement will include terms and conditions for the funding of the design, construction, and dedication of park, recreation and path facilities within the Lompa Ranch North Specific Plan area. The agreement will outline the City's process for the collection and distribution of Residential Construction Tax (RCT) compliant with CCMC 15.60. The agreement must be considered and approved by the Board of Supervisors prior to recording the Final Map.
 - b. The phasing plan will be modified to identify phase triggers and unit counts for park, recreation and path facilities for the Lompa Ranch North Specific Plan area. The phasing plan will be modified and approved by the Board of Supervisors prior to recording the Final Map.
 - c. A private Home Owner's Association (HOA), Landscape Maintenance District (LMD), or similar instrument will be established for the Lompa Ranch North Specific Plan area to provide for the operations and maintenance of all park, recreation and path facilities. Operation and maintenance standards for these facilities will be established by the City. The applicant will draft an agreement for the Board of Supervisor's consideration and approval no later than issuance of the certificate of occupancy for the 200th residential unit.
 - d. A private Home Owner's Association (HOA), Landscape Maintenance Association (LMA), or similar instrument will be formed to provide 100% funding and maintenance for all the following areas in perpetuity: Common landscape and open space areas, buffer areas between the development and neighborhoods, landscaping associated with the development's path system, landscape medians, street corridors, non-public recreation facilities/amenities, detention basins, and drainage channels. The maintenance and funding shall be addressed in the developer agreement to the satisfaction of the Board of Supervisors. Common area maintenance shall include at a minimum, but not limited to the following:
 - i. Debris, weed, and litter removal

- ii. Noxious and invasive weed management, including fire prevention
 - iii. Care and replacement of plant material
 - iv. Plant material irrigation and irrigation system repair
- e. As the development's phases are implemented, the plans will be submitted for review by the City. The applicant shall be required to demonstrate pedestrian connectivity between the neighborhood parks, "off-street/paved/shared" multi-use paths, and sidewalks. This shall be done to the satisfaction of the Parks, Recreation, and Open Space Department.
- f. All "off-street/paved/shared" multi-use paths and sidewalks will conform to the standards and policies outlined in of the Carson City Unified Pathways Master Plan adopted April 6, 2006 (as revised March 15, 2007) and as amended in the future. There will be adequate pedestrian connectivity, throughout the development that provides convenient and logical access to neighborhood parks and paths and enhances the overall sidewalk network within the development.
60. Neighborhood Park Comments (10 acre park / west of I-580)
- a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 400th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation and Open Space Department.
 - b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including water conservation design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.
 - c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 750th residential unit. Upon successful completion, final project acceptance of said work will be done to the satisfaction of the City, through its Parks, Recreation and Open Space Department.
61. Neighborhood Park Comments (3 acre park / east of I-580)
- a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 100th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation, and Open Space Department.
 - b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including water conservation design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.
 - c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 250th residential unit. Upon successful completion, final project acceptance of said work will

be done to the satisfaction of the City, through its Parks, Recreation, and Open Space Department.

62. Off-street/paved/shared Path Comments

1. Robinson Street and the Spine Road shall be constructed as full street improvements, to City standards and engineering requirements, including the construction of "on-street bike lanes" and concrete "off-street/shared/paved" multi-use paths. The path along Robinson Street will be constructed on the road's south side and the path along the Spine Road will be constructed on the road's east side.
2. All multi-use paths will be designed and constructed to a 10' wide (minimum) AASHTO standard concrete multi-use path (off street/paved/shared) with an adjacent 3' wide decomposed granite path.
3. The multi-use paths will include landscaping with a variety of trees (either evergreen or deciduous) that will be planted at a rate of 1 tree per 50 lineal feet (tree groupings are acceptable) with a minimum of 4 shrubs per tree.
4. Path amenities include but are not limited to park benches/seating areas (per 1000 lineal feet of trail along the path), pet waste stations/trash cans, signage depicting direction and trail distance.

Other comments

63. The applicant is required to use best management practices during construction to prevent the spread of noxious and invasive weeds and will incorporate language in construction documents to ensure contractors and subcontractors comply. The Parks, Recreation and Open Space Department will assist the applicant with this condition.

LEGAL REQUIREMENTS: CCMC 17.05 (Tentative Maps); CCMC 17.07 (Findings); NRS 278.330;

MASTER PLAN DESIGNATION: Lompa Ranch North Specific Plan; Medium Density Residential (MDR)

ZONING DISTRICT: Approved as Single Family-6000 square feet (SF6) - effective when the ownership of the land changes

KEY ISSUES: Is the Tentative Map consistent with the Specific Plan? Does the proposal meet the Tentative Map requirements and other applicable requirements? Will the City or public be materially injured by the approval of the associated abandonment?

SURROUNDING ZONING AND LAND USE INFORMATION

NORTH: Public (P) / Carson City High School and a Church

SOUTH: Approved as Multi-family Apartment (MFA) and existing Agriculture (A) / Vacant and a Single Family Residence and Agricultural Land

WEST: Single Family 6000 square feet (SF6) / Single Family Detached Residences

EAST: Approved as Single Family 6000 square feet (SF6) and existing Agriculture (A)/ Vacant and Ranch

ENVIRONMENTAL INFORMATION:

FLOOD ZONE: Zone AE and AO (100 year flood plain)

SLOPE/DRAINAGE: Generally flat

SEISMIC ZONE: Zone I (Severe) – No identified faults on or adjacent to the property

SITE DEVELOPMENT INFORMATION:

SUBJECT SITE AREA: 44.5 Acres

ZONING: SF6000 (minimum 6,000 square foot parcel size)

EXISTING LAND USE: Vacant Land

TOTAL RESIDENTIAL LOTS: 189 single family lots

PROPOSED LOT SIZES: Minimum Lot Size 6000 square feet (96 lots), Minimum Lot Size 7000 square feet (93 lots)

PROPOSED SETBACKS: Setback requirements for the SF6 zoning district apply

PARKING REQUIRED: Two spaces per dwelling unit

PROJECT PHASING: The proposed Tentative Map is the first phase of the Lompa Ranch Specific Plan. The phasing plan for the Specific Plan has 13 phases.

SITE HISTORY:

MPA-15-162: (March 17, 2016) Adoption of the Lompa Ranch North Specific Plan.

ZMA-15-163: (April 7, 2016) Adoption of a Zoning Map Amendment with an effective date of the date of a change of ownership.

CSM-16-188: (December 6, 2016) Review of a conceptual map for a 186 lot residential development.

BACKGROUND:

Consistent with Chapter 8 of the Master Plan, Lompa Ranch is one of four areas of the City that is subject to a Specific Plan designation. The Specific Plan designation requires development proposals within the area to be reviewed in a comprehensive manner. The policies contained in the specific plan provide a framework for development in the area.

The Lompa Ranch North Specific Plan, which encompasses 251.33 acres, was adopted on March 17, 2016. From a land use perspective, the Specific Plan includes residential areas of various densities, as well as commercial areas, areas for park land, and a ten acre area for a school and/or a fire station. The Specific Plan also addresses design standards, Parks, Open Space and Trails, Sanitary Sewer, Water Service, Storm Water Management, Utility Services, Roadways and Traffic, Fire Protection, Police Protection, and Schools.

With respect to infrastructure and public services, per the Specific Plan, a phasing plan needs to be completed prior to the first construction permit. As the subject tentative map represents the first phase of the Lompa Ranch development, and as the construction of the tentative map will constitute the first construction permit, the applicant has submitted a phasing plan with the Tentative Map. The phasing plan addresses storm water and drainage, traffic and roadways, water, and sanitary sewer planning for the entire 251 acre area. Staff has reviewed the proposed phasing plan, and has provided comments in the form of Conditions of Approval. Although these conditions are associated with the entire 251 acre specific plan areas, they are

included as recommended conditions of approval with the subject tentative map as the improvements associated with the tentative map will constitute the first construction permit.

By way of example, the subject property is in a 100 year floodplain. Significant drainage improvements, improvements that will cross “phasing lines” and require coordination are necessary for the ultimate buildout of the site. By identifying the improvements as part of the Specific Plan, the improvements will be implemented with each individual development that is considered by the City consistent with the timing identified in the approved phasing plan.

The Specific Plan also addresses parks, open space, and trail amenities, as well as fire mitigation. Per the Specific Plan, these improvements will be constructed by the developer and maintained via a homeowner’s association, landscape maintenance district or similar instrument. The ten acre park on the west side is to be constructed and dedicated to the City prior to the 750th residential unit west of Interstate 580. The three acre park on the east side of Interstate 580 is to be constructed prior to the 250 residential unit east of Interstate 580. Conditions regarding milestones to be met prior to completion of construction, including submittal of a parks conceptual plan at the time of the 400 residential unit to the west of Interstate 580, are intended to ensure the required improvements do not result in a construction delay, but rather are being planned concurrent with other construction activities.

The Specific Plan concludes that additional Fire facilities and equipment would be necessary to maintain existing levels of service once Lompa Ranch is built out. Section 3.8 of the Specific Plan calls out either the payment of a fee per dwelling unit and per 1000 square feet of nonresidential development, or, in lieu of the fee, implementation other mitigation measures. The Fire Department has identified City owned land on Butti Way as a desirable location for a fire station. The Fire Department finds that a fire station at this location with the appropriate apparatus will allow the existing level of service in the City to be maintained at the time of Lompa Ranch build out. It is the recommendation of the Fire Department that rather than collecting the identified fee and allocating land at Lompa Ranch for a Fire Station, the developer of Lompa Ranch construct a Fire Station sized to maintain the existing level of service on the City owned land on Butti Way. Fire Department staff is in discussions with the developer regarding the appropriate timing of the improvement given the realized impact.

As noted, the Phasing Plan must be approved prior to construction permit. The staff anticipates that the details of the Phasing Plan, as well as logistics associated with parks, open space, and trails and with the construction of the fire station will be memorialized in a development agreement. The development agreement will be subject to approval by the Board of Supervisors, and should be fully executed prior to the approval of the final map associated with the subject tentative map. The development agreement will be recorded so that all future property owners of the lands that comprise Lompa Ranch are aware of improvement and maintenance obligations.

DISCUSSION:

Blackstone Ranch Phase I is located east of South Saliman Road, south of East Robinson Street, and north of East 5th Street. Carson City High School is located to the north of the site, single family homes are located to the west of the site. There is a gas station, multi-family housing, and a ranch located to the south of the site. Vacant land that is part of the Lompa Ranch Specific Plan area is located to the east of the site. The subject property is currently zoned Agricultural (A), but is approved to be zoned Single Family 6000 square feet (SF6). The effective date of the approved zoning is the date the land ownership changes. Single family residential development is allowed by right in the SF6 zoning district.

The applicant proposes 189 residential lots, intended to contain detached single family residential homes. Ninety-six of the lots are proposed to have a minimum lot size of 6000 square feet, and ninety-three of the lots are proposed to have a minimum lot size of 7000 square feet. Although the applicant has submitted a conceptual plan depicting general fencing and landscaping features, the design standards of the Specific Plan for single family residential development, including fencing standards, landscaping, lighting, architectural elements, materials and colors, and massing and form will be reviewed at the time development applications for the individual lots are submitted. Vehicular access is proposed via South Saliman Road and East Robinson Street.

As noted, the subject property is located in the 100 year floodplain. Per the Specific Plan, a Letter of Map Revision must be approved by Carson City and submitted to the Federal Emergency Management Agency (FEMA) prior to submittal of the first construction permit. This has occurred. Additionally, prior to any construction permit being issued, the development must have a Conditional Letter of Map Revision (CLOMR) approved by Carson City and FEMA. Per the proposed conditions of approval, the flood conveyance channels identified in the CLOMR must be included with the improvement plans for the subject tentative map, and executed with the same. Bonding of improvements associated with the CLOMR will not be allowed.

PUBLIC COMMENTS: Public notices were mailed to 342 property owners within 900 feet of the subject site pursuant to the provisions of NRS and CCMC for the Tentative Subdivision Map application. As of the completion of this staff report, one letter expressing concerns with traffic and drainage has been received. Any additional written comments that are received after this report is completed will be submitted prior to or at the Planning Commission meeting on February 22, 2017 depending upon their submittal date to the Planning Division.

OTHER CITY DEPARTMENT OR OUTSIDE AGENCY COMMENTS: The following comments were received from City departments. Recommendations have been incorporated into the recommended conditions of approval, where applicable.

Engineering Division:

GENERAL: The Engineering Division has considered the elements of NRS 278.349, the Carson City Municipal Code and the Carson City Development Standards in its review of the tentative map described above.

This recommendation for 'approval with conditions' from the Engineering Division is based on conceptual level analysis that indicates the development as proposed will currently meet or will meet with concurrent improvements, prior to final map approval, Nevada Revised Statutes, the Carson City Municipal Code and the Carson City Development Standards. With the request for final approval of any and all phases, detailed engineering analysis addressing the following issues and recommending system improvements will be submitted to the Engineering Division.

FINDINGS: The following Conceptual Findings by the Engineering Division are based on approval of the subsequent conditions of approval:

1. *Environmental and health laws and regulations concerning water and air pollution, the disposal of solid waste, facilities to supply water, community or public sewage disposal and, where applicable, individual systems for sewage disposal.*

The existing infrastructure has been found sufficient to supply the water and sanitary sewer needs of the subdivision, and the City has the capacity to meet the water and sewer demand.

2. *The availability of water which meets applicable health standards and is sufficient in quantity for the reasonably foreseeable needs of the subdivision.*
The City has sufficient capacity to meet the water demand of the subdivision.
3. *The availability and accessibility of utilities.*
Water and sanitary sewer utilities are available and accessible.
4. *The availability and accessibility of public services such as schools, police protection, transportation, recreation and parks.*
The road network necessary for the subdivision is available and accessible.
5. *Access to public lands. Any proposed subdivision that is adjacent to public lands shall incorporate public access to those lands or provide an acceptable alternative.*
Development engineering has no comment on this finding.
6. *Conformity with the zoning ordinance and land use element of the city's master plan.*
Development engineering has no comment on this finding.
7. *General conformity with the city's master plan for streets and highways.*
With the conditions of approval offered below, the development will be in conformance with the engineering related requirements of the city's master plan.
8. *The effect of the proposed subdivision on existing public streets and the need for new streets or highways to serve the subdivision.*
The offsite improvements proposed for this subdivision are sufficient to meet the additional demand imposed by the subdivision.
9. *The physical characteristics of the land such as flood plains, earthquake faults, slope and soil.*
The site is currently in a FEMA floodway. Drainage channels proposed to be built with the subdivision will remove the floodway from the site and the surrounding area. Conditions of approval related to flood plain management and drainage must also be met.
10. *The recommendations and comments of those entities reviewing the subdivision request pursuant to NRS 278.330 thru 278.348, inclusive.*
Development engineering has no comment on this finding.
11. *The availability and accessibility of fire protection including, but not limited to, the availability and accessibility of water and services for the prevention and containment of fires including fires in wild lands.*
The subdivision has sufficient secondary access, and sufficient fire water flows.
12. *Recreation and trail easements.*
Development engineering has no comment on this finding.

RECOMMENDATION: If the tentative map is approved, the Development Engineering Division has the following recommended conditions of approval for the project:

A. Specific Conditions to be included in the Design of the Improvement Plans, to be met prior to approval of construction permit:

1. Two parking spaces must be provided per residence per Division 2.2 of the Carson City Development Standards. Parking spaces must not be tandem. If the driveway is used to count towards parking spaces, the driveway must be the appropriate length and width for the number of spaces per standard detail C-5.5.1
2. At the end of the cul-de-sac, at the southeast end of the development, additional drainage/access space must be provided to give City maintenance equipment sufficient space to turn around.
3. Underground storm drain systems that connect to flood conveyance channels shall do so completely above the base flood elevation of the channel.
4. Any changes to the Master Phasing Drainage Study must be reflected in changes to the drainage study for the subject subdivision.
5. Low Impact Design (LID) measures will be required to be implemented as part of the development storm drainage system.
6. The Conditional Letter of Map Revision (CLOMR) must be approved by FEMA prior to approval of any construction permits.
7. The CC&R's must clearly state that a Landscape Maintenance District (LMD) or a Home Owners Association (HOA) or similar entity is responsible for maintaining private storm drain infrastructure including any mains, basins, and LID infrastructure.
8. The minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
9. The irrigation diversion structure on the north side of E 5th St must be shown in the improvement plans and referenced in the technical drainage study for the subdivision.
10. The flood conveyance channels necessary for the CLOMR must be built with this subdivision. A sedimentation basin must be constructed as part of these improvements at the 90 degree turn of the Vicee channel.
11. Alexis Avenue must be renamed to make a continuation of Appaloosa.
12. Landscaping plans for the construction permit must include site distance triangles showing that sight distance is not inhibited.
13. Landscaping plans for the construction permit must show distances to existing and proposed water, sewer and storm drain mains to ensure a minimum of 10 foot spacing from trees.
14. A 10 foot wide multiuse pedestrian access path must be provided between the development and the future phase to the south.
15. The following street names cannot be used: Adam St, Maximus Ave, Alexis Ave, and Dianna St.
16. The utility plans for the construction permit must indicate precast manholes and bases. Cast in place manholes will not be allowed, regardless depth of new sewer mains. Riser depths must meet Carson City Standard Details.
17. A geotechnical report will be required for the subdivision prior to approval of any construction permits.
18. Local roads will have a minimum ACC pavement thickness of 4 inches.

B. Conditions to be Completed Prior to Submitting for Construction Permit or Final Map

19. A FEMA Conditional Letter of Map Revision (CLOMR), reflecting changes due to flood conveyance channels, must be approved by FEMA, prior to submitting plans for a construction permit. Any application submitted prior to FEMA approval of the CLOMR, will not be accepted.
20. All channels and associated access must be shown as separate parcels on the final map to be dedicated to the City.

C. Conditions Related to the Lompa Ranch Phasing Plan

21. The following change must be made to the master phasing plans and reports. The master phasing plans and reports must be included as part of a development agreement for the entire SPA area prior to approval of any construction permit or any tentative map acceptance, whichever comes first.
22. Note that geotechnical investigations will be required to establish design parameters for individual phases.

WATER:

23. The parallel water mains in the phasing plan must be shown as one single water main, however looping must be maintained.
24. The water infrastructure improvements exhibit must be updated to accurately reflect the new facilities required or predecessor to each phase. This column must indicate which mains are required to create looping for each phase. For instance, phase B1 calls for a main to be extended along Robinson Street, however a main would also need to be extended along the Spine Road from 5th Street in order to create a loop.
25. Note that no water main shall have more than 15 services without looping.

SEWER:

26. The sewer infrastructure improvements exhibit must note that cast in place manholes will not be allowed, regardless depth of new sewer mains, and that riser depths must meet Carson City Standard Details.

STORM DRAIN & FLOOD MANAGEMENT:

27. Plans must show and note that all flood channels (Vicee Canyon, Ash Canyon, and Kings Canyon) must provide sufficient access for City maintenance equipment along the full length, with access points spaced out no more than every 660 feet, and must note that Robinson St, the Spine Road, E 5th St, and N Saliman Rd are not to be considered part of this access.
28. Note that all flood channels and associated access must be on separate parcels to be dedicated to the City. Maintenance of these lands will be funded through an maintenance district or similar instrument, to be established prior to Final Map approval.
29. Note that privately owned and maintained LID/Water Quality facilities are required for each development.
30. The phasing drainage study must demonstrate the ability of downstream drainage facilities to handle increased runoff if detention is not used.
31. Note in the phasing plan that all structures must meet the Flood Protection Ordinance where the lowest floor is two feet above the base flood elevation of the FEMA 1% chance flood or the onsite 1% chance flood whichever is higher.
32. Note in the phasing plan that the minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
33. Note in the phasing plan that any crossings of flood channels must meet a 100-year flow capacity plus 18 inches of freeboard and must be a clear opening, no multi barrel pipes.
34. Note that drainage studies for all development phases shall demonstrate compliance

- with Floodplain Storage Capacity Protection requirements of CCMC 12.09.080 (9).
35. Note that drainage studies for all development phases shall provide emergency flow paths for a one hundred (100) year peak storm in accordance with Development Standards.
 36. The developer must design the Kings Canyon flood channel such that the drainage and/or any water rights associated with parcels 010-041-34 and 010-041-035 are not adversely affected.

TRAFFIC:

37. Note that Traffic Impact Studies required for all phases must demonstrate that the segment of N Saliman Rd between E William St and E Robinson St will have a projected level of service of C or better for year 2025 unless the North-South Spine Road is connected to William Street. The North-South Spine Road must connect to William Street prior to any development that would cause a level of service worse than C for this segment of road.
38. Note that Traffic Impact Studies required for all phases must demonstrate that the northbound leg and the westbound left turning movement of the N Saliman Rd/E William St intersection and the overall intersection will have a projected level of service of D or better for year 2025 unless the North-South Spine Road is connected to William Street. The North-South Spine Road must connect to E William Street prior to any development that would cause a level of service worse than D for the northbound leg or the westbound left turning movement of this intersection. Also note that traffic impact studies for each phase west of I580 will require traffic counts at this intersection.
39. Note that local roads will have a minimum Asphalt Concrete pavement thickness of 4 inches.
40. Phasing plan maps must be updated to show east-west connectivity between streets in phase A1 and phase B1, and a 10 foot multiuse path between phase A1 and phase A2.

D. General Conditions

41. Prior to the recordation of the final map for any phase of the project, the improvements associated with said phase must either be constructed and approved by the City, or the specific performance of said work secured by providing the City with a proper surety in the amount of one hundred fifty percent (150 %) of the engineer's estimate, recognizing that improvements associated with the CLOMR may not be secured and must be constructed. In either case, upon acceptance of the improvements by the City, the developer shall provide the City with a proper surety in the amount of ten percent (10 %) of the engineer's estimate to secure the Developers obligation to repair defects in workmanship and materials which may appear in the work within one year of acceptance by the City

PARKS AND RECREATION

1. General Comments
 - a. The applicant will enter into a developer agreement with the City. This agreement will include terms and conditions for the funding of the design, construction, and dedication of park, recreation and path facilities within the Lompa Ranch North Specific Plan area. The agreement will outline the City's process for the collection and distribution of Residential Construction Tax (RCT) compliant with CCMC 15.60. The agreement must be considered and approved by the Board of Supervisors prior to recording the Final Map.

- b. The phasing plan will be modified to identify phase triggers and unit counts for park, recreation and path facilities for the Lompa Ranch North Specific Plan area. The phasing plan will be modified and approved by the Board of Supervisors prior to recording the Final Map.
 - c. A private Home Owner's Association (HOA), Landscape Maintenance District (LMD), or similar instrument will be established for the Lompa Ranch North Specific Plan area to provide for the operations and maintenance of all park, recreation and path facilities. Operation and maintenance standards for these facilities will be established by the City. The applicant will draft an agreement for the Board of Supervisor's consideration and approval no later than issuance of the certificate of occupancy for the 200th residential unit.
 - d. A private Home Owner's Association (HOA), Landscape Maintenance Association (LMA), or similar instrument will be formed to provide 100% funding and maintenance for all the following areas in perpetuity: Common landscape and open space areas, buffer areas between the development and neighborhoods, landscaping associated with the development's path system, landscape medians, street corridors, non-public recreation facilities/amenities, detention basins, and drainage channels. The maintenance and funding shall be addressed in the developer agreement to the satisfaction of the Board of Supervisors. Common area maintenance shall include at a minimum, but not limited to the following:
 - i. Debris, weed, and litter removal
 - ii. Noxious and invasive weed management, including fire prevention
 - iii. Care and replacement of plant material
 - iv. Plant material irrigation and irrigation system repair
 - e. As the development's phases are implemented, the plans will be submitted for review by the City. The applicant shall be required to demonstrate pedestrian connectivity between the neighborhood parks, "off-street/paved/shared" multi-use paths, and sidewalks. This shall be done to the satisfaction of the Parks, Recreation, and Open Space Department.
 - f. All "off-street/paved/shared" multi-use paths and sidewalks will conform to the standards and policies outlined in of the Carson City Unified Pathways Master Plan adopted April 6, 2006 (as revised March 15, 2007) and as amended in the future. There will be adequate pedestrian connectivity, throughout the development that provides convenient and logical access to neighborhood parks and paths and enhances the overall sidewalk network within the development.
2. Neighborhood Park Comments (10 acre park / west of I-580)
- a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 400th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation and Open Space Department.
 - b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including

- water conservation design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.
- c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 750th residential unit. Upon successful completion, final project acceptance of said work will be done to the satisfaction of the City, through its Parks, Recreation and Open Space Department.
3. Neighborhood Park Comments (3 acre park / east of I-580)
 - a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 100th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation, and Open Space Department.
 - b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including water conservation design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.
 - c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 250th residential unit. Upon successful completion, final project acceptance of said work will be done to the satisfaction of the City, through its Parks, Recreation, and Open Space Department.
 4. Off-street/paved/shared Path Comments
 - a. Robinson Street and the Spine Road shall be constructed as full street improvements, to City standards and engineering requirements, including the construction of "on-street bike lanes" and concrete "off-street/shared/paved" multi-use paths. The path along Robinson Street will be constructed on the road's south side and the path along the Spine Road will be constructed on the road's east side.
 - b. All multi-use paths will be designed and constructed to a 10' wide (minimum) AASHTO standard concrete multi-use path (off street/paved/shared) with an adjacent 3' wide decomposed granite path.
 - c. The multi-use paths will include landscaping with a variety of trees (either evergreen or deciduous) that will be planted at a rate of 1 tree per 50 lineal feet (tree groupings are acceptable) with a minimum of 4 shrubs per tree.
 - d. Path amenities include but are not limited to park benches/seating areas (per 1000 lineal feet of trail along the path), pet waste stations/trash cans, signage depicting direction and trail distance.

Other comments

5. The applicant is required to use best management practices during construction to prevent the spread of noxious and invasive weeds and will incorporate language in construction documents to ensure contractors and subcontractors comply. The Parks, Recreation and Open Space Department will assist the applicant with this condition.

Building Division: No comments.

Fire Department:

1. Project must comply with the currently adopted fire code and applicable amendments adopted by Carson City.
2. Hydrant spacing as shown on Alexis Ave doesn't meet the 2012 IFC Appendix C spacing.
3. The cul-de-sac at the end of Achilles Street is too small. It must comply with the 2012 IFC Appendix D figure D103.1
4. The traffic circle mentioned in the traffic study comment 1/response 1 on the single lane traffic circle- the traffic circle must have at least 20' total width for travel lanes.

Environmental Control: Required compliance with CCMC Title 12.06 and Appendix 18 Division 15.5, and all applicable codes found in Chapters 7 and 10 of the 2012 Uniform Plumbing Code.

Health Department: No comments.

School District:

1. Traffic impact on Robinson Street
 - a. Regardless of the outcome of proposed new road exiting near Gold Dust West Casino, we feel that the traffic study likely is underestimating the traffic density that will come with the development. We believe it is underestimated because of the limited road access to the development area and because the "peak hours" considered in the study did not take into account the dismissal time for Carson High, let alone the expected impact of dismissal at an adjacent new school on Robinson Street.
 - b. The increased students coming from the development would also increase the number of buses and bus trips coming out of our District bus garage, which is located on Robinson, behind Carson High School.
 - c. This underestimation would lead to great safety concerns for both students and staff in the area, especially for students walking to and from school.
 - d. There would also be safety concerns for bus loading on the street.
 - i. We believe that the two suggested cross sections would not meet our needs.
 - ii. We could move the bus loading off of the street, but we have no incentive to consider that as we would incur substantial costs to construct an appropriate area off-street and we are happy with the current bus loading location and process.
2. New school-land availability and cost
 - a. The 10-acre parcel reserved for the school district would not become available until the 700th unit is sold. While we have more calculations to do on our side, but because it takes two years to build a school, we feel that may be a little late for our families' needs. We are currently near capacity at all of our schools and while we are certainly excited to see Carson City grow, this could pose a serious problem.
 - b. We believe the location adjacent to Carson High School to be a good one, we are continuing internal discussions to consider the wider impact.
 - c. Revenues to offset construction costs

- i. Carson City's population is just over 55,000, which precludes us from considering to levy a Residential Construction Tax to help offset the costs to the District of community growth.
 - ii. School Districts in the State of Nevada cannot collect Impact Fees to offset the costs of growth.
 - iii. We cannot levy a Washoe-styled new sales tax to fund the construction of capital items.
 - iv. We cannot collect property taxes for homes that don't yet exist. Collecting property taxes on 700 new homes (when the land is proposed to become available to us) would not generate the bonding capacity to build a new \$25,000,000 elementary school, let alone pay for the land to put under it. We are fully bonded out and with what appears to be an environment of increasing interest rates, we are very concerned.
 - v. Carson City was able to leverage a Condition of Development to provide a park and fire station as part of the development but the District does not have this leveraging authority nor ability.
3. Drainage
- a. Carson High School currently has problems with a drainage ditch on the northeast side of our property which will also become a concern for the Lompa development in that area.
 - b. Mr. Myers offered to meet with our staff and the Carson City Storm Water Chief in that location to discuss mutually beneficial options, which we greatly appreciate.
 - c. CCSD raised the suggestion of piping and burying the drainage ditch on CHS campus due to major safety concerns this year. Mr. Myers mentioned that, "all of our drainage concerns will be taken care of".

TENTATIVE MAP FINDINGS: Staff recommends approval of the Tentative Subdivision Map based on the findings below and in the information contained in the attached reports and documents, pursuant to CCMC 17.05 (Tentative Maps); 17.07 (Findings) and NRS 278.349, subject to the recommended conditions of approval, and further substantiated by the applicant's written justification. In making findings for approval, the Planning Commission and Board of Supervisors must consider:

1. ***Environmental and health laws and regulations concerning water and air pollution, the disposal of solid waste, facilities to supply water, community or public sewage disposal and, where applicable, individual systems for sewage disposal.***

The development is required to comply with all applicable environmental and health laws concerning water and air pollution and disposal of solid waste.

2. ***The availability of water which meets applicable health standards and is sufficient in quantity for the reasonably foreseeable needs of the subdivision.***

Water supplied to the development will meet applicable health standards. Carson City's water supply will not be exceeded by final approval of this development.

3. ***The availability and accessibility of utilities.***

All utilities are available in the area to serve this development.

4. *The availability and accessibility of public services such as schools, police protection, transportation, recreation and parks.*

The project is located within an existing neighborhood that is served by existing schools, sheriff protection, transportation facilities and parks. The proposed subdivision will not overburden police protection. As part of the area Specific Plan, parks and recreation facilities are required to be constructed and dedicated to the City so as to meet increased demands. The subject tentative map will be subject to a development agreement addressing the timing of construction and maintenance of parks, open space, and trails in Lompa Ranch.

The Lompa Ranch Specific Plan includes a ten acre land reservation for a school to meet future enrollment needs. The development team has met with school district staff to discuss traffic, drainage, and the school site. Per the school district staff, the proposed location next to the high school is likely appropriate, but they are continuing to evaluate the wider impact. School district staff is concerned about its ability to fund the acquisition and development of the land, but agrees that a new school, likely an elementary school, will be necessary due to on-going residential growth.

5. *Access to public lands. Any proposed subdivision that is adjacent to public lands shall incorporate public access to those lands or provide an acceptable alternative.*

The proposed subdivision is located within a developed area of Carson City that is not adjacent to public lands. Access is not required in this case.

6. *Conformity with the zoning ordinance and land use element of the City's Master Plan.*

The proposed subdivision is consistent with the Master Plan and the Lompa Ranch Specific Plan for permitted primary uses in the Medium Density Residential (MDR) land use designation. Primary uses in this land use area include single family homes.

7. *General conformity with the City's Master plan for streets and highways.*

Subject to compliance with the proposed conditions of approval, the proposed subdivision conforms to the City's master plan for streets.

8. *The effect of the proposed subdivision on existing public streets and the need for new streets or highways to serve the subdivision.*

The proposed tentative map will take access from East Robinson Street, across the street from the high school. Currently, and for over fifteen years, school buses have stacked along Robinson Street for morning drop off and afternoon pick up. While stacked, the buses are required to have flashing lights on, thus limiting travel along this portion of roadway. Expansion or shifting of the roadway away from the stacking would be difficult due to the location of a ditch along the south side of the road. The school campus has space to accommodate on-site bus parking, but no funds to make the associated improvements.

In order to allow for the functionality of East Robinson Street, staff recommends that the applicant work with the school district to create an off-street loading and drop off area on the existing campus. These improvements should be included in the improvements associated with subject tentative map, and completed prior to the recordation of the Final Map.

As previously noted, the subject Tentative Map is part of the Lompa Ranch Specific Plan. As part of the Specific Plan, a phasing plan addressing roadway improvements necessary for the entire buildout is required. This phasing plan has been submitted, and staff has identified required modification. These modifications are identified in the form of conditions of approval associated with the subject tentative map.

9. *The physical characteristics of the land such as flood plains, earthquake faults, slope and soil.*

The physical characteristics of the site currently do preclude the development as proposed. Per the specific plan, prior to the first construction permit, the development must have a CLOMR approved by Carson City and FEMA. The improvements associated with the approved CLOMR, per the proposed conditions of approval, must be constructed with the improvement plans associated with the subject project.

10. *The recommendations and comments of those entities reviewing the subdivision request pursuant to NRS 278.330 thru 278.348, inclusive.*

The proposed tentative map has been routed to the Nevada Department of Environmental Protection and the Nevada Division of Water Resources. The Division of Water Resources has requested that a Will Serve letter from Carson City Public Works and a final mylar map of the proposed project be presented to the State Engineer for approval and signed through his office prior to development. This request has been incorporated into the proposed conditions of approval.

11. *The availability and accessibility of fire protection including, but not limited to, the availability and accessibility of water and services for the prevention and containment of fires including fires in wild lands.*

As noted in the Specific Plan, the development of Lompa Ranch will negatively impact the Fire Department's level of service. To maintain the existing level of service, the Fire Department is requesting that the applicant construct a Fire Station on City owned land on Butti Way rather than collecting fees and allocating land on-site. The specific requirements regarding the sizing of the station and the timing of completion should be addressed as part of the Phasing Plan, to be agreed to prior to the issuance of the first construction permit, and memorialized in a development agreement prior to approval of a final map for the subject project.

12. *Recreation and trail easements.*

The Lompa Ranch Specific Plan specifically addresses Parks, Open Space, and Trails. The trail system is required to conform with the standards and policies of the Unified Pathways Master Plan. The Parks and Recreation staff has reviewed the tentative map for compliance with the requirements of the Specific Plan and finds, subject to the incorporation of the conditions of approval, the proposed tentative map will be in compliance.

The Specific Plan includes a requirement for the completion and dedication of a ten acre park at the time of the 750 dwelling unit on the west side of Interstate 580. The Specific Plan also obligates a homeowners association or a landscape maintenance district be formed for purposes of maintaining landscaped areas, open space, trails, and parks and recreation facilities. Staff has included in the conditions of approval various milestones to ensure progress is being made on the park planning so as to avoid disrupting development of the site.

Section 3.1.6.j of the Specific Plan states that the residential construction tax will not be collected at the time of building permit. The District Attorney's office has advised that the City must collect this tax. As is done in other developments in the City, the staff would recommend keeping funds collected from Lompa Ranch in a separate fund, and then be refunded to the developer at the time the parks are accepted by the City.

Staff recommends that conditions regarding park planning, as well as maintenance responsibilities and funding requirements be addressed in the development agreement.

Attachments

Lompa Ranch North Specific Plan Area, Adopted March 17, 2016
City and State Comments
Correspondence from Carson City School District
Public Correspondence
Tentative Map Application (TSM-16-023)

Lompa Ranch North Specific Plan



Photo Credit: aroundcarson.com

**ADOPTED
MARCH 17, 2016**

CARSON CITY BOARD OF SUPERVISORS

LOMPA RANCH NORTH SPECIFIC PLAN

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Lompa Ranch North Specific Plan

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1. Introduction

1.1 Location

The Lompa Ranch North Specific Plan Area encompasses 251.31± acres. The majority of land (203.27±) acres is located on the west side of Interstate 580, north of East Fifth Street, east of Saliman Road, and south of US Highway 50 (East William Street). The remaining 48.04± acres is located on the east side of Interstate 580 along the western side of Airport Road. Figure 1 (below) depicts the Lompa Ranch North in context with the surrounding area.



Figure 1 – Lompa Ranch North Specific Plan Area

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1.2 Purpose

The purpose of this Development Handbook is to provide for the orderly development of the Lompa Ranch North Specific Plan Area (SPA) as envisioned, while assuring that the stated desired level of quality is achieved. Since implementation of public and private improvements will occur in multiple phases, over many years, the standards and guidelines contained herein establish a common framework to guide individual improvement plans. The development of the property is controlled and restricted by these development requirements as well as by all applicable government codes and regulations. This Development Handbook is not intended to limit creativity or prevent variation necessary to respond to unique site conditions, but rather to generate consistency and quality throughout the SPA.

This SPA is for the Lompa Ranch North properties specifically identified with this document. Future development of the remaining Lompa Ranch properties as identified in the 2006 Carson City Master Plan shall be required to receive approval of a new SPA for those areas prior to development.

1.3 Vision

The Lompa Ranch North SPA is intended to provide for a sustainable community that includes a range of land uses that complement not only each other but those that currently exist outside of the SPA boundaries. The vision is to provide for a viable community that promotes a variety of housing types supported by well-balanced commercial, recreational, and educational opportunities.

Complementing the commercial uses and neighborhoods within Lompa Ranch North will be a linear open space preserve along Interstate 580 as well as a network of trails and sidewalks throughout the community, providing non-vehicular connectivity to the various internal and regional components of the area. Throughout Lompa Ranch North, consistent design themes, entries, and landscape treatments will establish a sense of place/community and recall the property's ranching roots.

1.3.1 Land Use Pattern

The land use mix within Lompa Ranch North provides for varying levels of compatible densities and intensities that will result in a synergy that attracts both residents and businesses. This supports walkability within the community to commercial, recreational, employment, and public activities. It also minimizes the consumption of land associated with traditional suburban development by encouraging and creating a more

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compact development pattern that is efficient for infrastructure, public services and maintenance.

1.3.2 Sense of Place and Community

Creating a sense of place is one of the key components in creating a vibrant and balanced community. A sense of place is fostered within Lompa Ranch North by creating human-scale environments in which the individual can feel both comfortable and safe. This includes provisions for open space and walking paths, neighborhood parks, common design themes, and uses that complement each other. Furthermore, the Lompa Ranch North SPA promotes and provides for connectivity between various neighborhoods and uses that are integrated through the standards included within this handbook.



1.3.3 Diverse Housing Mix



The Lompa Ranch North SPA provides for neighborhood diversity by allowing for a mix of residential densities and product types to support a wide range of resident interests and needs. The



densities included in the SPA will also support and complement planned commercial uses within the Lompa Ranch North plan area. Furthermore, this diversity in densities and housing types serves to break up the monotony of traditional residential development by reinforcing the dynamics of character and identity within each of the neighborhoods.



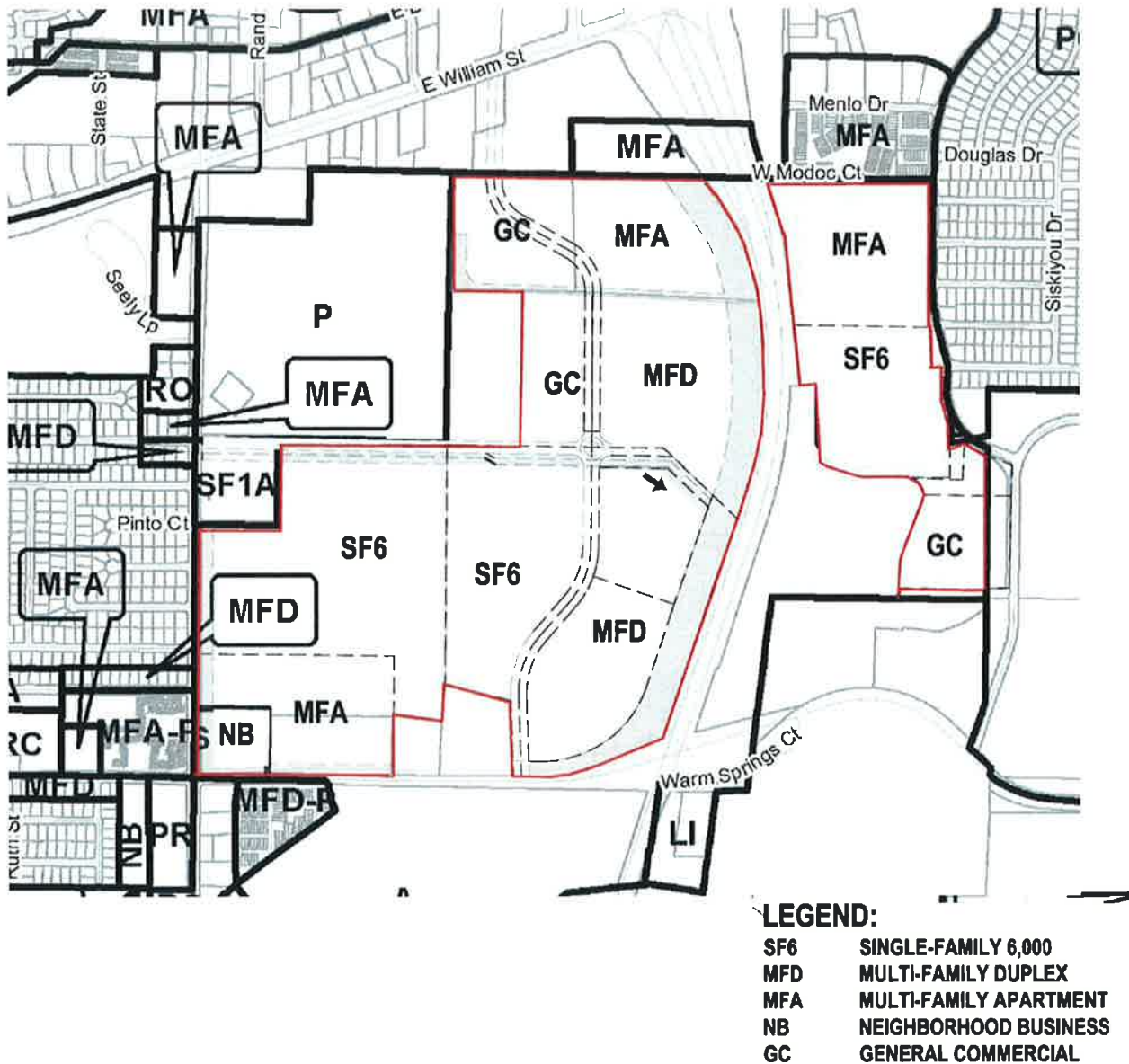
1.3.4 Implementation

This handbook will be used by the Carson City Community Development Department as a guide for reviewing individual projects within the boundaries of the Lompa Ranch North SPA.

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1.4 Allowed Uses

Allowed uses within the Lompa Ranch North SPA shall be determined based on the underlying zoning categories, as included in the Carson City Municipal Code Title 18. The zoning districts included within Lompa Ranch North are depicted below:



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Master Plan land use designations for the Lompa Ranch North SPA are included below:



1.4.1 General Standards

- The Lompa Ranch North SPA is envisioned to include a mix of residential uses ranging from 4 units per acre up to 36 units per acre.
- Land use is determined based on zoning. Zoning adopted with this Specific Plan shall be reviewed and approved by the Carson City Planning Commission and Board of Supervisors and deemed to be appropriate for the site(s).

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- c) Commercial uses at a varying range of intensities are encouraged within the SPA to serve both new residents of Lompa Ranch North as well as those within the surrounding area. Commercial uses shall be located as to properly relate to adjoining uses.

- d) Uses within Lompa Ranch North shall conform to the underlying zoning district(s) assigned to the individual parcels as outlined in Title 18 of the Carson City Municipal Code

- e) Supplemental review required for specific uses within zoning categories such as Special Use Permits shall remain in effect per the Carson City Municipal Code (refer to allowed uses within individual zoning categories).

- f) This Specific Plan shall not grant any special privileges or waivers in terms of public review or entitlements otherwise required under the Carson City Municipal code in terms of allowed uses or supplemental review.

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2 Standards and Guidelines

The site planning standards and guidelines address general provisions of site development which include building orientation, grading and drainage, parking areas, landscape, lighting, signs, walls and fences, and service areas. Site planning controls the proper placement of buildings and internal roads that service and access the various land uses in the community. It addresses the linkages and land use relationships at a human-scale, in order to create a stimulating and visually pleasant community. The goal is to promote pedestrian activity and safety, create visual compatibility with surrounding neighborhoods and minimize negative impacts on the natural environment. These standards are intended to be used in addition to the standards outlined in the Carson City Municipal Code, Title 18 Appendix - Development Standards. In cases where a conflict exists, the stricter of the standards shall apply. Where these standards are silent, the Carson City Development Standards shall apply.

2.1 Commercial Uses

2.1.1 Commercial Site Planning Standards

- a) Building placement and orientation shall be designed to create visual interest along public streets. Multiple buildings in a single project shall demonstrate a positive functional relationship to one another.
- b) To the extent possible, buildings located within a single project shall be clustered. Plazas and pedestrian areas shall also be an important element in the design of clustered buildings. When clustering is impractical, a visual link should be established between buildings through the use of architectural features, landscaping, etc.
- c) For general commercial uses, a minimum of 15 percent of the building area should be located at or near the front setback line. This minimizes large, continuous areas of parking and encourages active streetscapes.
- d) Buildings shall be oriented so that public access or windows face adjoining streets.
- e) Plazas or common areas within a project shall be located near building entrances or areas of high pedestrian traffic to ensure their use.
- f) To the extent possible, areas between buildings shall be utilized for plazas, outdoor seating, or landscape

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features in order to eliminate “dead zones” of underutilized space.

g) Bicycle racks shall be provided within all commercial centers.

2.1.2 Commercial Grading and Drainage

a) Design of commercial uses shall be sensitive to the natural terrain, and structures should be located to minimize necessary grading and preserve natural site features such as drainageways, wetlands, etc. Grading of commercial sites should blend with the natural topography of the site.

b) Grading within commercial areas shall be designed to complement the architectural and landscape design character of the center and surrounding area. Grading techniques can be used to screen parking and service areas, reduce the perception of height and mass on larger buildings, and provide reasonable transitions between uses.

c) Graded slopes should properly transition to existing natural terrain at project borders.

d) Man-made slopes shall not exceed an average of 3:1 slope and turf areas shall not exceed an average 4:1 slope.

e) Areas disturbed by grading activities shall be revegetated prior to the issuance of a certificate of occupancy. If climatic conditions or other circumstances prevent planting at the time of occupancy, a bond shall be provided for landscaping during the subsequent growing season. Drought tolerant plant species shall be utilized to help minimize erosion.

f) New commercial developments must include a final hydrology report to be reviewed and approved by the Carson City Engineering Department prior to the issuance of a building permit.

g) An erosion control plan shall be included with each grading permit.

Appendix 1 contains the Conceptual Drainage Study and Stormwater Management Report for Lompa Ranch North.

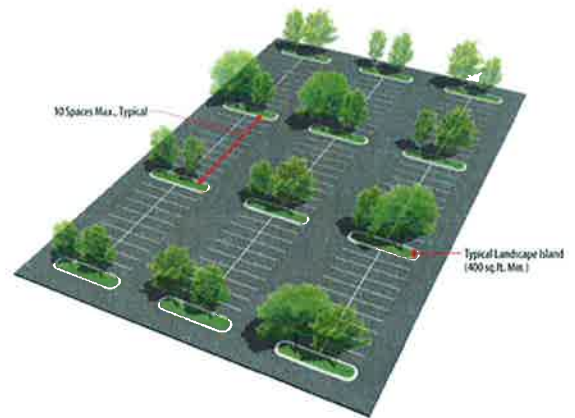
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2.1.3 Commercial Parking Lots

a) A minimum of 10 feet of landscaping shall be provided between parking lots and the public streets.

b) A minimum 400 square foot interior planter shall be provided at the end of parking aisles (refer to example to the right). Planters shall include a minimum of one deciduous tree (min. two inch caliper) – see example to right.

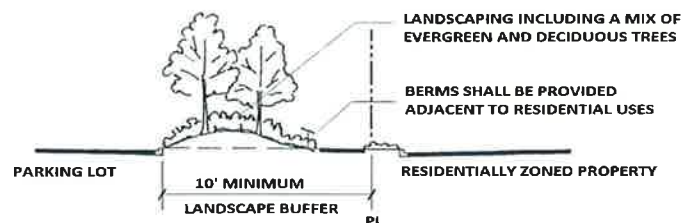
c) Landscape islands (minimum of 400 square feet) shall be provided for every 10 spaces in large parking fields and shall include a minimum of one deciduous tree (two inch caliper minimum). See example to right.



d) Pedestrian connections between parking lots and buildings shall be provided along with connections to sidewalks along adjoining public streets.

e) Parking should be located to the side and rear of a project site where feasible. However, no more than 10 percent of the required parking shall be in the rear service area (typically not used for general public access) of a project site.

f) Parking areas shall be screened from adjoining residential areas through the use of landscaping and berming. This buffer shall be a minimum of 10 feet in width (see example to right).



g) Commercial centers that include tenants that utilize shopping carts shall provide a “cart corral” within 150 feet of 85 percent of their parking stalls.

h) For commercial centers exceeding 5 acres, a maintenance plan shall be required for parking lots that includes regular sweeping and a snow removal/storage plan for winter weather events.

i) For commercial centers adjoining residential areas, parking lot sweeping shall be limited to the hours

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between 8:00 am and 9:00 pm.

j) Parking lot design, including space dimensions, aisle widths, etc. shall comply with the provisions of the Carson City Municipal Code.

k) Outdoor sales or special events may not reduce parking past minimum requirements mandated in the Carson City Municipal Code.

2.1.4 Commercial Landscaping

a) Landscaping, including plant materials and themes shall be consistent throughout the Lompa Ranch North SPA.

b) Landscaping standards contained in the Carson City Development Standards shall apply within Lompa Ranch North.

c) Within commercial centers, areas not utilized for parking, buildings, plazas, or access/circulation shall be landscaped to the back of curb. Unbuilt pad areas shall be excluded from this standard.

d) Drought tolerant plantings shall be used in conjunction with low water demand principles and techniques.

e) All landscaped areas shall be irrigated with permanent automatic irrigation systems. All irrigation systems shall be placed underground.

f) Landscape maintenance within commercial areas shall be the responsibility of individual property owners or completed through a private maintenance association.

g) Landscaping along adjoining rights-of-way shall be a minimum width of 15 feet and provide a mix of trees, shrubs, and living groundcover. Trees shall be provided at a rate of 1 tree per 25 lineal feet of street frontage with a minimum of six shrubs per tree

2.1.5 Commercial Lighting

a) Adequate lighting shall be provided to ensure a safe pedestrian environment.

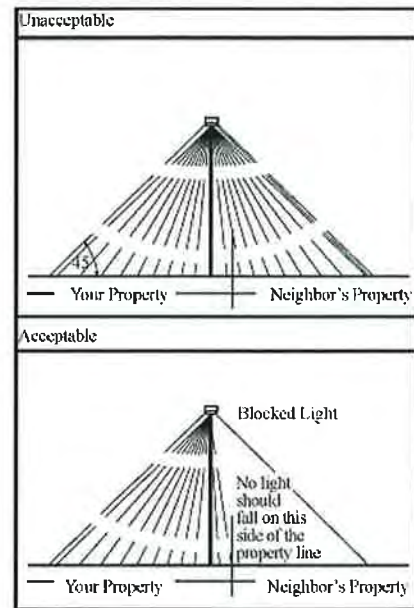
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b) Parking lot lighting within 75 feet of residential areas shall be limited to 12 feet in height and shall incorporate shielded fixtures. Additional height limitations for parking lot lighting within certain distances of residential areas are identified in the Carson City Development Standards.

c) Parking lot lighting shall use shielded/directed fixtures to ensure that spill-over and glare do not occur on adjoining properties. See example to right.

d) The use of bollard lighting is encouraged in pedestrian areas.

e) Exterior lighting shall be used for purposes of illumination and safety only, and shall not be designed for, or used as, an advertising display.



2.1.6 Commercial Signs

Signs and their integration into the project is a critical element in the design of Lompa Ranch North. Careful use of forms, styles, materials, and colors will establish continuity throughout the community. Signs are intended to be utilized only where necessary, and in an understated manner, emphasizing an image of permanence and quality.

a) Signs shall be included on facades or entry canopies of buildings and illuminated or backlit with indirect lighting. All tenant identification signs shall be consistently located and integrated into the architectural design of the building entry. Storefront signs shall be proportional with the building architecture (see example to right).



b) Flashing or animated signs are prohibited.

c) Building signs that project more than 4 inches beyond the wall façade are prohibited, unless incorporated as an architectural element.

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d) Hanging signs may be included under eaves above walkways and shall maintain a minimum of 8 feet of clearance. These signs shall be architecturally compatible with the building they serve (see example to right).



2.1.7 Commercial Fencing

a) Walls and fences shall be utilized to provide a buffer between incompatible uses. It is important, however, that walls are appropriately integrated into each project

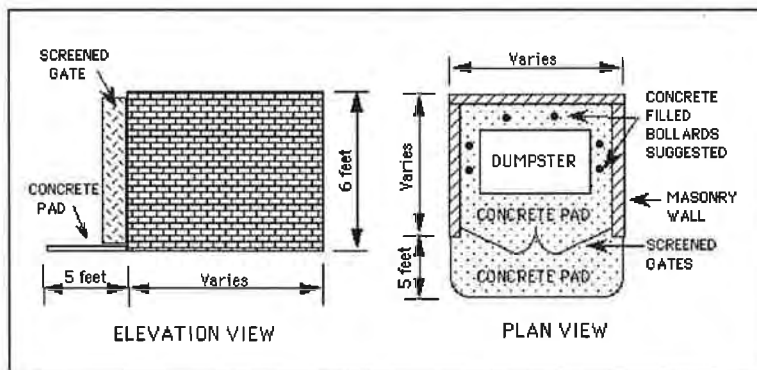
b) Solid fencing (6 foot minimum) shall be installed between commercial uses within Lompa Ranch North and adjoining residential uses. This can include wood or vinyl fencing, concrete block walls, pre-cast wall systems, or similar.

c) Chain link fencing shall be prohibited within commercial centers.

2.1.8 Commercial Trash and Utility Areas

a) Service and maintenance areas shall be screened from adjacent public right-of-ways, pedestrian plazas or adjacent residential uses with landscaped berms, walls or plantings. Storage areas shall be enclosed by a 100% site obscuring fence or wall, permanently installed and maintained at a minimum height of six feet.

b) All trash and garbage bins shall be stored in an enclosure that includes solid screening, to the approval of the Carson City Community Development Department.



c) Trash enclosures shall incorporate building materials, colors, etc. that are complementary to the overall project architecture. Gates shall be constructed of durable building materials that screens at a minimum 80% of the view into the trash enclosure. Wood or chain link gates are not allowed (see example to left).

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d) Trash enclosures must include provisions for concrete pads or appropriately designed asphalt sections in front of the enclosure. The area in front of the trash enclosure shall be a minimum of six (6) feet to reduce pavement damage from disposal trucks.

2.2 Single Family Residential Areas

2.2.1 Neighborhood Diversity

Single family areas within the Lompa Ranch North SPA will include varied densities and housing types in order to create separate and distinct neighborhoods within the project. This can be accomplished through the use of varied housing types, distinct architectural styles and elements, etc.

a) Densities within single family areas will range from 3 to 8 dwelling units per acre.

b) Neighborhood density shall properly relate to adjoining developed areas and provide for transition between neighborhood types. Proper transitions can include feathering of density/lot size, landscape buffers, or walls/fences that serve to identify community boundaries.

c) Individual single family projects within the SPA boundary may create their own sense of identity through the use of entry features that include distinctive signage, entry treatments, landscape improvements, water features, etc.

d) Varied densities are encouraged throughout the SPA boundary to encourage varied product types including single family detached homes, patio homes, duplexes, townhouses, etc. Additionally, new urbanism design principles such as house-forward designs with residential alleyways are permitted within the single family areas.

e) It is the intent of the SPA to provide a number of distinctly different neighborhood types rather than a single "large neighborhood" with a single product type.

f) Variation in architectural styles is encouraged throughout the SPA in order to provide distinct neighborhood identity to new subdivisions within the Lompa Ranch North.

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2.2.2 Single Family Neighborhood Design

Neighborhoods within Lompa Ranch North will promote quality development that is complementary to the existing built environment, while establishing its own sense of identity through uniform and innovative design. A variety of single family detached, as well as single family attached products are anticipated within the SPA boundary.

a) To the extent possible, “forward” architecture shall be used in the design of homes. This is accomplished by placing entries, windows, front porches, and living areas towards the street on most plan variations.

b) With the exception of zero lot line lots, plans should be reversed and plotted so that garages and entries are adjacent to each other. This creates an undulating sense of setback. Occasionally this pattern should be broken so that it will not become overly repetitious or reflected by the massing across the street.



c) The garage shall not be the dominant feature of the building facade facing the street and should be offset through architectural detailing for garage forward elevations.

d) So as not to contribute to a repetitious and monotonous appearance along the street, the use of varying building setbacks from the street right-of-way is encouraged.

e) Neighborhoods shall provide connections into the community trail system.

g) In order to avoid a “walled-in” feel, homes backing to parks, open space, or drainage corridors shall include open rear fencing. This includes the use of split rail or iron fencing. See example to right.

h) Setbacks for single family residential areas shall comply with the underlying zoning district for which the subdivision is located. In order to provide for visual interest within the streetscape, front setbacks may be reduced up to 5 feet in order to achieve a non-monotonous/repetitive streetscape pattern.

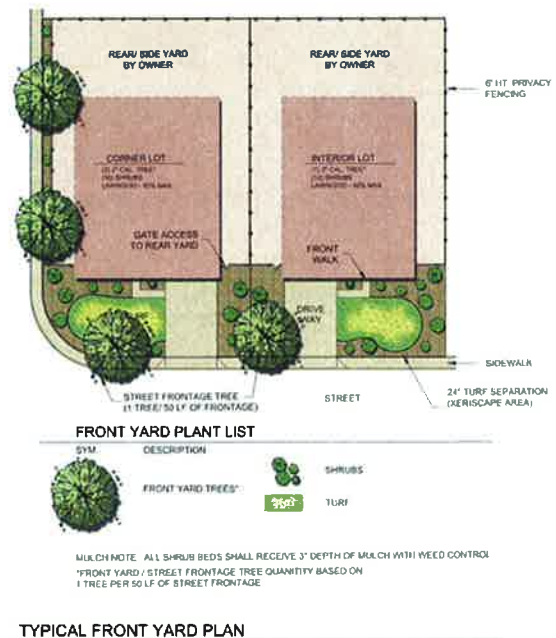
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2.2.3 Single Family Grading

- a) The design of residential neighborhoods shall be sensitive to the natural terrain, and structures shall be located in such a manner so as to minimize necessary grading and preserve natural site features and drainage ways. Any grading of the site terrain shall blend with the natural topography of the site.
- c) Graded slopes shall be rounded resulting in smooth, harmonious transitions between the man-made terrain and the natural terrain.
- d) All graded slopes shall be revegetated prior to building occupancy. If climatic conditions or other circumstances prevent planting at the time of occupancy a bond shall be provided for landscaping during the subsequent growing season or other arrangements made for revegetation, subject to the approval of the administrator. Drought tolerant plant species shall be utilized to help minimize erosion.

2.2.4 Single Family Landscaping

- a) Front and street side yard landscaping shall be installed by the builder prior to the occupancy of the individual home. See example to right.
- b) Front yard landscape packages shall provide for a minimum of 1 tree per 50 lineal feet of street frontage as well as a minimum of 12 shrubs. Trees shall be a minimum of 1 inch caliper for deciduous and 6 feet in height for evergreens. Shrubs shall be a minimum of 2 gallon.
- c) Xeriscape options for front yards shall be permitted. Xeriscape packages must include the required trees and shrubs outlined under the previous standard.



- c) Front yard landscaping is required for all homes and will be reviewed and approved with the tentative map establishing installation timing.
- d) Front yard landscape packages shall include an automatic irrigation systems.

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2.2.5 Single Family Lighting

- a) Lighting shall be designed to differentiate land use areas, emphasize community amenities, provide continuity along street corridors and ensure the safety of residents and users.
- b) Exterior lighting shall be shielded from projection offsite and designed to be compatible with the architectural and landscape design of the home.

2.2.6 Single Family Walls and Fencing

- a) Walls may be used where necessary to provide privacy and security for residential neighborhoods when adjacent to arterial or collector roadways, or when adjoining non-residential uses.
- b) Walls within the community shall not become the dominant visual element and walls where needed shall blend into the overall landscape.
- c) Walls within Lompa Ranch North shall not exceed 6 feet in height. Acceptable materials include stone, stone veneer, split face/precision block, slump stone, and stuccoed CMU.
- d) Open fencing shall be used where the rear of individual lots are adjacent to open space. See examples below.
- e) Open fences at rear yards may include landscaping with trees and shrubs to screen views of private yards from adjacent properties, common areas, and/or roadways.
- f) Acceptable open fencing materials include wood or vinyl split-rail or wrought iron. See examples below.



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g) Single family residential lots may include solid privacy fences. Acceptable materials include wood and vinyl. Privacy fencing shall not exceed 6 feet in height.

h) Chain link fencing is prohibited within residential areas.

2.3 Multi-Family Residential Site Planning

2.3.1 Multi-Family Building Orientation

a) Multi-family structures should be grouped in clusters of buildings rather than one large continuous structure in order to minimize the scale of the project.



b) Open space areas and courtyards shall be created within multi-family developments in order to break up building mass and provide recreational opportunities. See example to left. Open space/recreational areas shall be provided per the requirements of the Carson City Municipal Code.

c) To provide privacy between living spaces, there should be distance separations, buffering or changes in the angles of units. See examples below.



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d) All multi-family/attached single family developments shall incorporate pedestrian connections to adjoining residential, recreational and commercial uses as well as to the community trail system (where practical).

e) Multi-family/attached single family projects in excess of 35 units shall provide a secure children's play area. Additionally, such projects shall incorporate a minimum of 5 recreational facilities. These can be any 5 of the following:

- Swimming pool
- Tennis courts
- Horseshoe Pits
- Spa
- Fitness Center/Gym
- Game room
- Community room
- Picnic areas to include tables with barbecues
- Volleyball court
- Basketball court



f) Recreation facilities shall be conveniently and centrally located for the majority of the units (see examples to right).



g) Private open space, such as decks or patios, shall be contiguous to the units with a minimum width of six (6) feet.

h) Setbacks shall conform to the underlying base zoning. Deviations to setbacks within 10% of requirements may be granted by the Carson City Community Development Director or his/her designee.

2.3.2 Multi-Family Grading and Drainage

a) The design of multi-family housing or attached single family housing shall be sensitive to the natural terrain, and structures shall be located in such a manner so as to minimize necessary grading and preserve natural site features and drainage ways. Any grading of the site terrain shall blend with the natural topography of the site.

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b) Site grading shall be designed to complement the architectural and landscape design character of the community, screening parking and service areas, reducing the perception of height and mass on larger buildings, and providing reasonable transitions between on-site uses.

c) Graded slopes shall be rounded resulting in smooth, harmonious transitions between the man-made terrain and the natural terrain.

d) All graded slopes shall be revegetated prior to building occupancy. If climatic conditions or other circumstances prevent planting at the time of occupancy a bond shall be provided for landscaping during the subsequent growing season or other arrangements made for revegetation, subject to the approval of the administrator. Drought tolerant plant species shall be utilized to help minimize erosion.

Appendix 1 contains the Conceptual Drainage Study and Stormwater Management Report for Lompa Ranch North.

2.3.3 Multi-Family Parking

a) Parking areas shall not be located in excess of 400 feet from individual units within multi-family projects.

b) Pedestrian links between units (i.e. sidewalks) shall be provided between all units and parking areas.

c) Garages and covered parking shall be designed as an integral part of the architecture of the development and shall include the same colors, materials, etc. as the primary building(s). Carports should not have roof pitch of less than 3:12.

2.3.4 Multi-Family Landscaping

a) Minimum landscape requirements shall be established by the Carson City Development Standards based on underlying zoning of the project site.

b) Drought tolerant and low water demand plantings shall be used to the extent possible. Xeriscaping may be substituted for turf areas and must contain trees and shrubs per the standards of the Carson City Development Standards.

c) Automatic irrigation systems shall be installed with all multi-family projects. All irrigation systems shall be placed underground.

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- d) Large parking lots (in excess of 25 spaces) within multi-family shall provide a minimum 400 square foot landscape island containing at least one tree (two inch caliper) for every 10 spaces of required parking.
- e) Landscaping along adjoining rights-of-way shall be a minimum width of 15 feet and provide a mix of trees, shrubs, and living groundcover. Trees shall be provided at a rate of 1 tree per 25 lineal feet of street frontage with a minimum of six shrubs per tree.

2.3.5 Multi-Family Lighting

- a) The height of lighting within multi-family projects shall be in scale with the setting and complement the architecture. Light fixtures over 10 feet shall include a cut-off shield to prevent the light source from being directly visible from off-site areas.
- b) Light sources shall be kept as low to the ground as possible while ensuring safe and functional levels of illumination. For example, the use of bollard lighting rather than pole lighting is required in pedestrian areas. See examples below.



- c) Illumination of landscape features or building facades for aesthetic purposes shall ensure that light does not project beyond the project boundary.

2.3.6 Multi-Family Walls and Fencing

- a) Multi-family projects that adjoin common areas, open space, or drainageways shall include open fencing adjacent to such features. Acceptable materials include wood or vinyl split rail or wrought iron and shall not exceed 6 feet in height.

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b) In areas where open fencing is employed, landscaping shall be used to screen views of private yards from adjacent properties and public streets.

c) Design of all walls and fences shall be consistent in terms of material, color and detail within each multi-family and attached single family residential project. Chain link fencing is prohibited.

d) In areas where multi-family development adjoins either single family residential or commercial use, a minimum 6-foot wall shall be provided for separation. Acceptable materials include stone, stone veneer, split face/precision block, slump stone, and stuccoed CMU.

2.3.7 Multi-Family Service and Utility Areas

a) Enclosures shall be provided in order to screen all trash dumpsters and shall architecturally complement the primary building(s). Enclosures shall include solid gates and screen a minimum of 80% of the interior area. See example to right.



b) Trash enclosures shall include durable materials that complement the primary architecture and shall be screened with landscape on three sides and shall comply with the Carson City Development Standards. Chain link fencing is prohibited. See example to right.

c) The use of individual trash cans for multi-family projects in excess of 15 units shall be prohibited.

2.4 ARCHITECTURE STANDARDS AND GUIDELINES

2.4.1 Architectural Theme

It is the intent of the Lompa Ranch North SPA to promote a high quality development that incorporates an architectural style that reflect the historical ranching aspect of the area. Therefore, a ranch and craftsman architectural theme is adopted with the Lompa Ranch North SPA.

Variations on the ranch/craftsman style are encouraged in order to promote creative design, innovative features, and high quality elevations. Variations may include the introduction of a southwestern elements

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such as barrel tile roofs or Victorian elements such as wrap-around porches. These deviations will be complementary to the overall theme and can add visual interest within the community.

2.4.2 Residential Architectural Elements

a) New structures within Lompa Ranch North shall, at a minimum, incorporate a minimum of two of the following elements:

- Gable roofs with deep overhangs.
- Exposed rafters, brackets, columns, etc.
- Decorative doors and windows
- A mixture of 2 (at a minimum) exterior elements including stucco, wood siding or shingles, brick, or stone
- Exterior porches or courtyards

b) Acceptable roofing materials include concrete or clay tile, slate, or architectural grade (30+ year) composition asphalt shingles. Metal roofing may be used as an architectural element in conjunction with the previously listed materials.

c) Flat roofs are prohibited in residential areas.

d) Metal buildings, other than accessory sheds not to exceed 250 square feet, are prohibited.

e) Modular homes are not permitted within the Lompa Ranch North SPA.

f) Building articulation shall include a minimum of 4 separate roof planes incorporated on front/primary elevations. Front/primary elevations shall contain a minimum of 2 wall planes offset by a minimum of 3 feet.

g) Building colors shall utilize an earth tone pallet such as browns, tans, whites, greens, deep reds and oranges, pale yellows, etc. The use of bright or vibrant colors is prohibited with the exception of highlighting architectural elements.

2.4.3 Commercial Architecture

Commercial areas within the Lompa Ranch North SPA are envisioned to complement residential uses in function and form. Smaller retail uses will incorporate the ranch theme while larger commercial centers can

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take a more traditional center approach with the inclusion of the ranch theme elements such as rock, stone, brick, etc.

2.4.4 Commercial and School Building Mass and Form

a) Individual buildings, forms, and components within commercial centers shall be designed as a whole to ensure unity to the overall design of the center.

b) Facades shall include articulation to ensure that the large scale of commercial buildings is softened and appropriate for the area at a human scale. Articulation shall be provided on all sides of any commercial building that is adjacent to a public right-of-way or main commercial parking area.

c) Visual interest shall be created in building facades through the incorporation of wall plane projections or recesses that are a minimum of two (2) feet in depth.

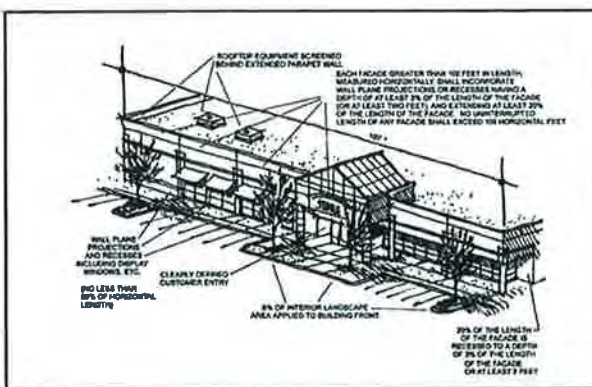
d) Wall plane projection or recess may be substituted with a combination of vertical or horizontal elements such as trellises, awnings, shed roofs, or columns. Any such element must have a minimum of 2 feet change in vertical or horizontal projection or recess. The proposed alternative design solution shall meet the intent of this standard.

e) In commercial areas adjoining residential uses, building heights shall relate to the adjacent development to enhance view corridors and ensure compatibility.



f) Multi-tenant commercial spaces shall use color change, texture change, material change, or relief change to avoid large expanses of blank walls and box-like structures (see example to the left).

g) Buildings in excess of 10,000 square feet should vary building and roof forms to give the appearance of smaller forms. See example to right.



h) Commercial centers that include multiple buildings shall incorporate a consistent architectural theme. Pad site buildings with conflicting architectural style are prohibited.

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2.4.5 Commercial Roof Form

a) Rooflines shall include variations to add visual interest and reduce the scale of large buildings. Refer to example below.



b) Roof profile elements visible at ground level shall incorporate horizontal and vertical offsets as depicted in the example above.

c) All rooftop equipment shall be screened from public view at street level and the parking lot.

d) All roof mounted mechanical equipment must be screened from public view at the street level and the parking lot.

2.4.6 Commercial Materials and Colors

a) The colors and materials of new buildings shall be compatible with those of adjoining buildings/uses.

b) Exterior building materials shall be of high quality. These may include, but are not limited to:

- brick
- stained, painted, or weathered wood/cementitious products such as heavy timbers or stock lumber
- stone veneer/cultured stone
- integral color split face block or rough cut wood
- metal such as corrugated, battened or standing panelized systems; performed painted or stained metal shapes
- fabric or metal awnings
- dimensioned asphalt or simulated wood shingles
- tilt-up concrete with wood texture, or other similar treatment

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- c) Accent colors (including vibrant colors) may be used to emphasize special façade elements in order to attract attention at focal points.
- d) Facades shall include the use of earth tone palette colors in broad expanses. The use of high intensity colors, very dark colors or fluorescent colors are discouraged unless they are used to accentuate architectural forms or features.
- e) Building trim and accent may feature a brighter, more intense palette of colors used to direct focus toward important building elements.
- f) The following exterior building materials are not allowed as predominant features on building facades:
- integral color smooth-faced or painted concrete masonry
 - tilt-up concrete panels without textures or finishes
 - pre-fabricated steel panels
 - unprotected wood
 - dimensional asphalt shingles (architectural grade asphalt shingles may be used on roofs)

2.4.7 Single Family Residential Architecture

Architectural standards for residential areas promote an upscale development concept that reflects a western and ranching heritage while providing for modern amenities and features. Although neighborhoods may include distinctive architectural designs, common elements serve to create a cohesive community that creates a sense of place.

2.4.8 Single Family Building Mass and Form

- a) Home facades shall incorporate the architectural style and materials outlined in section 2.4.1.
- b) A minimum of 3 distinctive floor plans shall be used within each subdivision. Subdivisions with less than 20 lots are exempt from this requirement. Phasing of 20 units or less does not circumvent this standard.
- c) Architectural details and stylings used on the front of the home shall be carried over to all elevations.
- d) A minimum of 3 distinctive front elevations shall be included for each model within subdivisions. Matching elevations shall not be allowed to repeat next to each other.
- e) Varied setbacks, floorplans, and elevation packages shall be used within subdivisions to create a visually interesting streetscape.

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2.4.9 Single Family Roof Form

a) Roof planes are required to vary through the use of architectural features such as dormers, gables, hipped roofs and variations in pitch appropriate to the homes chosen architectural style.

2.4.10 Single Family Materials and Colors

a) As mandated within other provisions of this handbook, single family homes shall incorporate an[~~e~~] earth tone color palette. The use of bright and vibrant colors is prohibited with the exception of enhancing key architectural elements and features.

b) Conflicting architectural styles within a single subdivision shall be prohibited.

c) Building materials and elements shall be consistent with those outlined under previous standards.

2.4.11 Single Family Garages

a) Garages shall include a minimum of 5 feet offset from inhabitable areas. Front elevations should provide focus on living areas and not garages.

b) Home plans shall incorporate one of the garage designs listed below and each subdivision shall incorporate at least two of these techniques to reduce the emphasis of the garage on the street (see examples to left).



- Recessing garage back a minimum of five (5) feet in relationship to the front of the house.



- Incorporation of a side-load garage that eliminates the continuous view of garage doors from the street.

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c) Garage forward plans shall be permitted when offsets (5 feet minimum) exist for the garage in order to provide visual distinction between the garage and residence. See examples below.



2.4.12 Multi-Family Architecture

Multi-family standards are intended to result in a visually pleasing product that does not reflect a “big box” appearance and incorporates elements to break up building masses, provide articulation at a human scale, and complement single family uses within the Lompa Ranch North SPA.

2.4.13 Multi-Family Building Mass and Form

a) Facades of multi-family buildings shall be articulated using at least one of the architectural elements previously listed in the Architectural Theme standards.



b) Buildings shall incorporate facade articulation with no long expanses of flat wall planes, vertically or horizontally, exceeding 50 feet (see example to left).

c) Architectural elements (i.e., exterior materials, fenestration, window trims, cornices, arches, etc) shall be utilized on all sides of the building.

d) Architectural elements such as towers, piers and varied rooflines may be used to break up the horizontal massing and provide visual interest.

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e) Single family attached products such as townhomes that include garages and/or carport are more than 50 percent of the total width of the unit shall incorporate architectural features such as shutters, garage door window trim and minimum offsets of 2 feet, to reduce the visual impact of garages and carports on the front façade.

f) Garages and carports not attached to the main residential building shall match the main structure in building design, materials, roof pitch and architectural character.

2.4.14 Multi-Family Roof Form

a) Roofs planes shall include variation which can be accomplished with the inclusion of elements such as dormers, gables, hipped roofs and variations in pitch. (See example to right).

b) Roof materials shall include concrete tile, clay tile, slate, or architectural grade (30+ year) composition shingles. Metal roofing is prohibited as a primary material but may be used as an accent feature when combined with the allowed materials.



2.4.15 Multi-Family Materials and Colors

a) As mandated within other provisions of this handbook, multi-family uses shall incorporate an earth tone color palette. The use of bright and vibrant colors is prohibited with the exception of enhancing key architectural elements and features.

b) Varied elevations may be used within a single project. However, conflicting architectural styles within a single multi-family development shall be prohibited.

c) Building materials and elements shall be consistent with those outlined under previous standards.

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3 Public Services and Infrastructure

3.1 Parks, Open Space, and Trails

The Lompa Ranch North SPA envisions a community that is linked together through a system of trails, open space, and parks. The intent of these standards is to implement the provisions of the *Unified Pathways Master Plan*; *Parks and Recreation Master Plan*; and *Open Space Master Plan* adopted by Carson City.

3.1.1 General Standards

- a) A Landscape Maintenance District (LMD) shall be formed by the Master Developer to provide for the maintenance and upkeep of open space and common area landscaping, trails, and park/recreation facilities and amenities. The LMD shall be in place prior to the issuance of the first certificate of occupancy.
- b) A private homeowner's association (HOA) shall provide for the maintenance of all private landscape features and non-public recreation facilities (i.e. private parks within gated communities, etc.).
- c) Design of open space areas shall follow the standards and policies of the Carson City Open Space Plan, adopted by Carson City in June 2000.
- d) Pathways and trails, other than those described in Section 3.1.2 (following) shall conform to the standards and policies of the Unified Pathways Master Plan adopted by Carson City on April 6, 2006 (as revised March 15, 2007).
- e) Any new park facilities within the Lompa Ranch North SPA shall conform to the *Parks and Recreation Master Plan* as adopted by Carson City on April 6, 2006.
- f) Sidewalk connections to the Lompa Ranch North SPA trail/pathway system shall be provided in order to provide convenient and logical access to the trail system, parks/recreation areas, and open space.

3.1.2 Trails and Pathways

- a) Trails, pathways, and sidewalks not specifically called out within this section shall conform to the standards outlined in Section 6 of the Carson City Unified Pathways Master Plan (Pathway Types).

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b) As individual subdivisions and/or projects are submitted for review, the applicant/developer shall be required to demonstrate that trail connectivity between parks, trails, open space, and the high school is being provided. This shall be to the satisfaction of the Community Development and Parks, Recreation and Open Space Departments.

c) The trails and pathway system shall be constructed of concrete with a portion of it constructed using stabilized decomposed granite.

3.1.3 West Side Facilities

The following standards apply to properties within Lompa Ranch North that lie west of Interstate 580:

a) Prior to the issuance of the certificate of occupancy for the 750th residential unit west of Interstate 580, the Master Developer shall construct and dedicate to the City a minimum 10-acre neighborhood park site on the west side of the freeway as shown on the adopted land use map. This shall be coordinated through and agreed upon by the Carson City Parks, Recreation and Open Space Department.

b) For the park area west of the freeway, a meandering path (consistent with Unified Pathways Master Plan Standards) shall be constructed along a north/south route, connection 5th Street to the northern boundary of the SPA area. This pathway may follow a proposed drainage channel(s) where feasible and shall meet the guidelines for an “off-street/multi-use trail.” A multi-use path shall connect to the SPA’s park/recreation facilities in this project.

c) A fitness course may be substituted for park benches along the north/south trail. See examples below:



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- d) An east-west off-street multi-use path shall be constructed on the freeway's west side of the Lompa Ranch North SPA along 5th Street and connected to the east side development. Timing of this trail along with final alignment shall be in conjunction with new development and coordinated through the Parks, Recreation, and Open Space Department.
- e) An east-west multi-use path shall connect with the north/south trail, as depicted in the Unified Pathways Master Plan and described in c) and d) above.
- f) For park area west of the freeway, trails, pathways, and sidewalks shall provide off-street connectivity from 5th Street to Carson High School and Robinson Street.

3.1.4 East Side Facilities

The following standards apply to properties within Lompa Ranch North that lie east of Interstate 580:

- a) The Master Developer shall work with the Carson City Parks, Recreation and Open Space Department and provide for a 3-acre minimum neighborhood park site on the east side of Interstate 580 as depicted on the land use plan. The park site shall be constructed and dedicated to the City prior to the issuance of the certificate of occupancy for the 250th residential unit located on the east side of I-580. This shall be coordinated through and agreed upon by the Carson City Parks, Recreation and Open Space Department.
- b) For park area east of the freeway, the north/south trail being constructed by the City shall, at a minimum, include landscaping and pedestrian amenities. Trees (either evergreen or deciduous) shall be planted at a rate of 1 tree per 50 lineal feet with a minimum of 4 shrubs per tree. Park benches shall be located along the trails at a rate of 1 bench per 500 lineal feet of trail along with mileage parkers at one-mile intervals.
- c) The City property (approximately .13 acres) adjacent to the 3-acre minimum neighborhood park site shall be included in the park's design and constructed with the other park amenities.

3.1.5 Open Space

- a) All identified wetland areas within the Lompa Ranch North SPA shall be preserved as dedicated open space.
- b) Drainage channels shall be incorporated into open space areas and include trails/paths as described in section 3.1.2.
- c) Open space areas shall be maintained through a LMD and/or by a private homeowners association(s).
- d) Landscape medians, parkways, corridors, etc. included within common or open space areas shall be maintained by a private homeowners association(s) and/or through the LMD.

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e) Open space areas that remain private shall not include public access (if privately owned) and shall be maintained by a private homeowners association and not through an LMD.

3.1.6 Parks – General Standards

a) Parks within the Lompa Ranch North SPA shall be maintained through implementation of a Landscape Maintenance District. Any private parks (without general public access) shall be maintained by a private homeowners association(s).

b) Opportunities for joint use of park and open space facilities (i.e. stormwater detention basins) shall be a priority within the Lompa Ranch North SPA. This includes the incorporation of one or more dog park facilities.

c) All park facilities and open space areas shall have access to the overall trails/pathways system and sidewalk network within the SPA area.

d) Smaller public parks are discouraged within the SPA in favor of larger community parks. Private small parks or pocket parks may be permitted within individual subdivisions but shall be maintained by a private HOA, not the LMD.

e) Park facilities within Lompa Ranch North will be coordinated with the Carson City Parks, Recreation, and Open Space Department for review and approval as individual projects within the Lompa Ranch North SPA are brought forward.

f) Park design shall be consistent with Carson City Parks, Recreation, and Open Space Department guidelines and design standards, including water conservation design elements.

g) Playgrounds within public parks shall be designed to be universally accessible per design standards adopted by the Carson City Parks, Recreation and Open Space Department.

h) As part of the overall Lompa Ranch North park plan , provisions for a neighborhood “Splash Pad” and/or water play feature shall be included to the approval of the Carson City Parks, Recreation and Open Space Department.

i) New parks shall be designed to allow for automobile access, including City maintenance vehicles and emergency services.

j) The Master Developer, at its cost, will dedicate land and improvements for two neighborhood parks, detention basin parks and trails/pathways within the Project; as a result, the residential construction tax described in Carson City Municipal Code 15.06 – Residential Construction Tax et. seq. will not be collected by Carson City at the time building permits are issued for residential dwellings in the Project.

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3.2 Sanitary Sewer

a) All new development within the Lompa Ranch North SPA shall be required to connect to municipal sanitary sewer service.

b) Prior to submittal for the first construction permit, a complete description of all phasing must be submitted. This phasing description must indicate the geographical boundaries of each phase, a description of the proposed development for each phase, and the estimated sewer demand imposed by each phase. A final sewer report demonstrating capacity to serve the development shall be submitted with each individual project within the SPA boundary. Any existing sewer capacity provided to the development shall be on a “first come, first served” basis. There may be additional future infrastructure or costs associated with serving the development depending on build out time frames.

c) The site has no known constraints which would impact the ability to be served by a gravity fed extension of the public sewer. Existing sewer manholes in the SPA have vents. Any sewer vents located within or near the boundary of a phase must be evaluated as part of the sewer analysis for that phase to prevent odor issues.

d) An overall water and sewer technical report for each phase shall be submitted to and approved by Carson City prior to submittal for the first construction permit of each phase, to ensure that each project phase is properly sized and designed. The Lompa Ranch North Water and Sewer Demand Report is included as Appendix 5 of this document. Water and sewer technical reports shall include analysis of downstream/offsite capacities. Technical reports shall cite sources of any rate of demand used.

3.3 Water Service

a) All new development within the Lompa Ranch North SPA shall be required to connect to municipal water service.

b) Prior to submittal for the first construction permit, a complete description of all phasing must be submitted. This phasing description must indicate the geographical boundaries of each phase, a description of the proposed development for each phase, and the estimated water demand imposed by each phase. All new development shall be required to pay applicable water connection fees and demonstrate that adequate water supply is available to serve the project and dedicated for use. Any existing water capacity provided to the development shall be on a “first come, first served” basis. There may be additional future infrastructure or costs associated with serving the development depending on building out time frames.

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c) Separate irrigation meters will be employed in accordance with the guidelines present at the time of connection.

d) An overall water and sewer technical report for each phase shall be submitted to and approved by Carson City prior to submittal for the first construction permit for each phase, to ensure that each project phase is properly sized and designed. The Lompa Ranch North Water and Sewer Demands Study is included as Appendix 4 of this document. Water and sewer technical reports shall include analysis of downstream/offsite capacities. Technical reports shall cite sources of any rate of demand used.

3.4 Storm Water Management

The Lompa Ranch area benefits from extensive review and policy implementation that has been performed by Carson City as part of their long-range planning and infrastructure management processes. It is a goal of this Specific Plan to adhere to and complement this planning work. Policy *LR-SPA 3.1 Floodplain and Drainage*, from the Carson City Master Plan is therefore included in this document as a means of establishing long-range storm water management planning for Lompa Ranch North. This policy states:

- *The existing floodplain shall be identified based on FEMA mapping with post-freeway drainage improvements for development of the final SPA. In order to develop the property, drainage improvements will be required to mitigate the 100-year floodplain on the property. This may also require amending the FEMA mapping through a letter map amendment process. Once the new floodplain is determined, designated land use intensities shall be developed outside this floodplain area.*
- *An overall storm water management plan shall be developed with the final SPA to ensure adequate drainage facilities to serve the entire SPA area.*
- *A detailed wetlands delineation shall be provided with the final SPA identifying any areas that meet the Federal 404 definition of wetlands. Following wetland identification, designated land use intensities shall be developed outside the wetlands.*

Per the above policy, a wetlands delineation is currently planned for Spring 2016. The completion deadline for this task is June 30, 2016. No development shall occur within the Lompa Ranch North SPA until the wetlands delineation has been completed.

Additional resources for guiding storm water management (and other utilities) are the Conceptual Drainage Study and Stormwater Management Report for Lompa Ranch North (included in Appendix 1). In particular, this report states the following:

Lompa Ranch North Specific Plan

Based on the floodplain analysis, it is recommended that a LOMR be pursued based on the existing topography. The LOMR would remove much of the Lompa Ranch from the burden of delineated floodway both upstream and downstream of the Highway 395. It would establish discharges which could be used for the design of proposed drainage improvements including the design of channels along 5th Street, Saliman Drive, Robinson Road and north of Carson High School. In addition the model could be used for future site development planning and design and would be considered as the effective model for future modeling efforts, specifically those that would be part of a CLOMR for new development.

The existing *Master Plan Policy LR-SPA 3.1* and the *Conceptual Drainage Study and Stormwater Management Report* therefore form part of the standards for the Lompa Ranch North SPA.

The LOMR must be approved by Carson City and submitted to the Federal Emergency Management Agency (FEMA) prior to submittal for the first construction permit. Prior to any construction permit being issued, the development must have a conditional letter of map revision (CLOMR) approved by Carson City and FEMA. If the property is divided and sold to different owners, each separate development in the floodplain must have a CLOMR approved by FEMA prior to any construction permit being issued. The developer of any parcel in the flood plain, prior to any construction permit being issued, must provide funds to the City to process a Letter of Map Revision (LOMR) after the improvements are complete.

Additional standards include:

- a) The primary channels provided along Robinson Street, Saliman Road, Interstate 580, and 5th Street shall be designed to contain the existing off-site watershed discharges as well as the existing discharges from the SPA area.
- b) Onsite retention and detention facilities are required within the development of multi-family and commercial parcels.
- c) Existing drainage patterns shall be maintained.
- d) A comprehensive drainage impact analysis for the overall Lompa Ranch North SPA shall be reviewed and approved with the first tentative map and/or permit request. The analysis shall provide estimates of project impacts at buildout along with required upgrades, improvements, etc. as well as with triggers for when these improvements are required.
- e) Updates to the master drainage analysis shall be provided for any project proposing multi-family or commercial uses.
- f) Prior to submittal for the first construction permit, a complete description of all phasing must be submitted. This phasing description must indicate the geographical boundaries of each phase, a description of the proposed development for each phase, and the estimated stormwater runoff imposed by each phase.

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g) The development shall implement Low Impact Development (LID) Standards to include the *Truckee Meadows Low Impact Development Standards Handbook*, current edition, or current LID standards adopted by Carson City.

Appendix 1 contains the Conceptual Drainage Study and Stormwater Management Report for Lompa Ranch North.

3.5 Utility Services

a) All utility services within the Lompa Ranch North SPA shall be undergrounded. Overhead power lines shall be prohibited.

b) Plans for electrical, natural gas, telephone, and cable service shall be reviewed and approved by the applicable purveyor (i.e. NV Energy, Southwest Gas, AT&T, etc) prior to the issuance of a building permit.

3.6 Roadways

A traffic impact study has been completed for Lompa Ranch North (included in Appendix 2). This study includes recommended roadway improvements that mitigate the projected impacts. These roadway improvements are included below under their relevant heading.

a) All roadways within the Lompa Ranch North SPA shall comply with the standards and requirements included within the Carson City Municipal Code. This includes the provision of sidewalks where appropriate. All sidewalks in the Lompa Ranch North SPA shall be designed to provide connectivity to multi-use paths, parks, and open space.

b) Prior to submittal for the first construction permit, a complete description of all phasing must be submitted. This phasing description must indicate the geographical boundaries of each phase, a description of the proposed development for each phase, and the estimated traffic impact imposed by each phase.

c) An easement agreement or right of way must be in place prior to approval of any construction permits which are part of a phase which requires roadway improvements which will need additional right-of-way to be completed.

d) Each phase will require a traffic impact study to be completed and submitted for that phase prior to approval of any construction permits in that phase. The traffic study for Phase 1 will require coordination with the School District to mitigate impacts along Robinson Street.

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3.6.1 Saliman Road

a) Consistent with the conclusions/recommendations outlined in the traffic impact analysis (Appendix 2), add northbound dual lefts at E. William/Saliman intersection, and add northbound right turn lane at 5th Street. Include drainage improvements. Channel section to include open space for multi-use path.

3.6.2 Robinson Street

a) Robinson Street shall be improved to collector standards established by the Carson City Municipal Code. Robinson Street should be extended to intersect with a new north-south "spine road" within the project area and as shown in Exhibit 2. Robinson Street can be constructed with one through lane in each direction. Include drainage improvements. Channel section to include open space for multi-use path.

b) Consistent with the conclusions/recommendations outlined in the traffic impact analysis (Appendix 2), add westbound right turn lane at Saliman Road, and widen Robinson Street to accept dual left turn lanes from Saliman Road.

3.6.3 Fifth Street

a) Fifth Street shall include new drainage improvements to address site development conditions to the satisfaction of the Carson City Engineering and Public Works Departments.

b) Consistent with the conclusions/recommendations outlined in the traffic impact analysis (Appendix 2), add an intersection where the new Spine Road will meet 5th Street with an eastbound left turn lane, westbound right turn lane, southbound exclusive left and right turn lanes, and signalization (signalization only if warranted). Widen 5th Street at this intersection to accommodate turn lanes. Also, add a westbound right turn lane at Airport Road. Add a westbound right turn lane at Saliman Road, which may already be warranted without the project.

3.6.4 Airport Road

a) Right-turn lanes will be added along Airport Road based on the recommendations included in the reviewed and approved traffic impact analysis. The Carson City Engineering Department shall determine compliance with this standard.

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b) US 50/Airport – Consistent with the conclusions/recommendations outlined in the traffic impact analysis (Appendix 2), Provide northbound dual left turn lanes.

3.6.5 North/South Collector (Spine Road)

a) A collector roadway (Spine Road) shall be constructed from 5th Street extending north to US Highway 50. This road shall be designed as a limited access collector (per City standard) and include additional space for a multi-use path and landscaping, separated from vehicular traffic. The Spine Road can be constructed with one through lane in each direction. For Phase 1, the spine road may need to extend north of the Robinson Street extension.

b) US 50/Gold Dust Casino – Consistent with the conclusions/recommendations outlined in the traffic impact analysis (Appendix 2), add a northbound right turn lane and widen the south leg to accept a new left turn lane from westbound E. William Street. The south leg will continue to connect with the proposed north-south spine road.

c) Consistent with the conclusions/recommendations outlined in the traffic impact analysis (Appendix 2), a new three- to four-leg intersection at Robinson Street/Spine Road should be constructed to provide a north leg at this intersection. This north leg is proposed to continue to its connection with the south leg of the William Street/Casino intersection. This will require widening the existing south leg of this intersection to a standard two to three lane cross section.

d) The preferred northern intersection of the spine road is at the existing signalized intersection on William Street serving access to the Gold Dust Casino. The south leg of this intersection should be widened to accommodate a potential additional westbound to southbound left turn lane at this intersection. The spine road is anticipated to carry approximately 12,000 vehicles per day at Build Out. This volume approaches the threshold for a four-lane roadway. Further analysis and continuing discussions with the property owners south of William Road will be required.

3.6.6 U.S. 50/E. William Street

a) Consistent with the conclusions/recommendations outlined in the traffic impact analysis (Appendix 2), add westbound dual left turn lanes at the new Spine Road.

3.7 Traffic Impacts

a) A generic traffic impact analysis for the overall Lompa Ranch North SPA has been reviewed and accepted with this Specific Plan. This analysis provides estimates of the project impacts at buildout along with required upgrades, improvements, etc. Additional traffic impact studies will be required for each phase of development prior to approval of any construction permits which are part of that phase.

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b) Updates to the master traffic impact analysis shall be provided for any project generating more than 80 peak hour trips to determine if roadway upgrades/improvements are triggered. Such updates shall also address long-term cumulative impacts from the site as a whole so that appropriate refinements may be made to any mitigation measures.

Appendix 2 contains the Traffic Impact Study for Lompa Ranch North.

3.8 Fire Protection

The Carson City Fire Department currently services the Lompa Ranch North area from Fire Station # 1 located on Stewart Street. As development occurs within the Specific Plan boundary and surrounding area(s), an additional facility and/or equipment may be needed in order to ensure adequate levels of service for new development. As such, the following standards are included within this SPA:

a) As individual projects and subdivisions are submitted, the Carson City Fire Department shall review development plans in context with existing service limitations to ensure adequate levels of service are maintained.

b) The Carson City Fire Department has the ability to condition projects to ensure adequate levels of service are maintained for Lompa Ranch North. Such conditions include requiring fire sprinklers for new homes if response times are below accepted levels, inclusion of fire resistant building materials, requiring upgrades to existing equipment or purchase of new equipment, etc.

c) In order to assist in funding new fire facilities within the area (i.e. fire station), individual builders within Lompa Ranch North shall work with the Carson City Fire Department to participate in a program implemented by Carson City which provides funds (to be paid at time of building permit) that are dedicated to fire improvements. In the absence of a current City-wide impact fee program, fees shall be as follows for Lompa Ranch North: a minimum of \$1,000.00 per dwelling unit in single family or multi-family residential development. Also, a minimum fee of \$1,000.00 per 1,000 square feet of business, industrial, commercial or lodging facilities. The Board of Supervisors reserves the right to use this fee to offset the cost to the City of other facilities that is incurred as a result of the impacts of the proposed development.

d) In lieu of and as an alternative to the fire fee, it may be possible for individual builders within Lompa Ranch North to work with the Carson City Fire Department to determine if other mitigation measures may be available. Such measures could include, but are not limited to, providing improvements such as paving, utility extensions, etc. along with construction of new facilities, etc. These improvements shall be credited

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back to any applicable fire fee. This shall be reviewed on a case by case basis dependent on current Fire Department needs and demands.

e) New development within Lompa Ranch North shall participate in any applicable impact fee program that is enacted by Carson City. This SPA shall not exempt development from any impact fee program adopted post-approval of this SPA.

3.9 Police Protection

The Carson City Sheriff's Department currently operates patrols in the area. The following standards related to police protection are provided for the Lompa Ranch North SPA:

a) All new projects submitted for review by Carson City shall be routed through the Sheriff's Department for review and comment.

b) The Sheriff's Department shall reserve to the right to condition projects in order to implement and or incorporate crime prevention measures, etc.

c) New commercial projects within Lompa Ranch North shall be required to submit a lighting and security plan to the Sheriff's Department for review and approval.

3.10 Schools

The following standards have been developed in conjunction with the Carson City School District:

a) A new elementary school site (minimum of 10 acres) shall be reserved within Lompa Ranch North to meet future enrollments needs.

b) The elementary school site shall be made available prior to the issuance of the 700th residential certificate of occupancy.

c) Generally, the 10-acre elementary school site should be located on the west side of Interstate 580, central to the project site near the current terminus of Robinson Street.

c) All residential development within the Lompa Ranch North SPA shall be required to provide estimated

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student enrollment projections to the Carson City School District for review.

d) The Master Developer of the Lompa Ranch North SPA shall work with the School District to participate in the current (2016) School Facilities Master Plan Update process to ensure that needs identified within the SPA boundary are addressed.



MEMORANDUM

DATE: February 7, 2017
TO: Hope Sullivan – Planning Division
FROM: Stephen Pottéy P.E. – Development Engineering Division
RE: TSM 17-005 Blackstone Lompa Ranch Phase A1 Subd.
Engineering Text for Planning Commission Staff Report

The following text is offered for inclusion in the Planning Commission staff report for the above referenced land use proposal:

GENERAL: The Engineering Division has considered the elements of NRS 278.349, the Carson City Municipal Code and the Carson City Development Standards in its review of the tentative map described above.

This recommendation for 'approval with conditions' from the Engineering Division is based on conceptual level analysis that indicates the development as proposed will currently meet or will meet with concurrent improvements, prior to final map approval, Nevada Revised Statutes, the Carson City Municipal Code and the Carson City Development Standards. With the request for final approval of any and all phases, detailed engineering analysis addressing the following issues and recommending system improvements will be submitted to the Engineering Division.

FINDINGS: The following Conceptual Findings by the Engineering Division are based on approval of the subsequent conditions of approval:

- 1. Environmental and health laws and regulations concerning water and air pollution, the disposal of solid waste, facilities to supply water, community or public sewage disposal and, where applicable, individual systems for sewage disposal.*
The existing infrastructure has been found sufficient to supply the water and sanitary sewer needs of the subdivision, and the City has the capacity to meet the water and sewer demand.
- 2. The availability of water which meets applicable health standards and is sufficient in quantity for the reasonably foreseeable needs of the subdivision.*
The City has sufficient capacity to meet the water demand of the subdivision.
- 3. The availability and accessibility of utilities.*

Water and sanitary sewer utilities are available and accessible.

4. *The availability and accessibility of public services such as schools, police protection, transportation, recreation and parks.*
The road network necessary for the subdivision is available and accessible.
5. *Access to public lands. Any proposed subdivision that is adjacent to public lands shall incorporate public access to those lands or provide an acceptable alternative.*
Development engineering has no comment on this finding.
6. *Conformity with the zoning ordinance and land use element of the city's master plan.*
Development engineering has no comment on this finding.
7. *General conformity with the city's master plan for streets and highways.*
With the conditions of approval offered below, the development will be in conformance with the engineering related requirements of the city's master plan.
8. *The effect of the proposed subdivision on existing public streets and the need for new streets or highways to serve the subdivision.*
The offsite improvements proposed for this subdivision are sufficient to meet the additional demand imposed by the subdivision.
9. *The physical characteristics of the land such as flood plains, earthquake faults, slope and soil.*
The site is currently in a FEMA floodway. Drainage channels proposed to be built with the subdivision will remove the floodway from the site and the surrounding area. Conditions of approval related to flood plain management and drainage must also be met.
10. *The recommendations and comments of those entities reviewing the subdivision request pursuant to NRS 278.330 thru 278.348, inclusive.*
Development engineering has no comment on this finding.
11. *The availability and accessibility of fire protection including, but not limited to, the availability and accessibility of water and services for the prevention and containment of fires including fires in wild lands.*
The subdivision has sufficient secondary access, and sufficient fire water flows.
12. *Recreation and trail easements.*
Development engineering has no comment on this finding.

RECOMMENDATION: If the tentative map is approved, the Development Engineering Division has the following recommended conditions of approval for the project:

A. Specific Conditions to be included in the Design of the Improvement Plans, to be met prior to approval of construction permit:

1. Two parking spaces must be provided per residence per Division 2.2 of the Carson City Development Standards. Parking spaces must not be tandem. If the driveway is used to count towards parking spaces, the driveway must be the appropriate length and width for the number of spaces per standard detail C-5.5.1
2. At the end of the cul-de-sac, at the southeast end of the development, additional drainage/access space must be provided to give City maintenance equipment sufficient space to turn around.
3. Underground storm drain systems that connect to flood conveyance channels shall do so completely above the base flood elevation of the channel.
4. Any changes to the Master Phasing Drainage Study must be reflected in changes to the drainage study for the subject subdivision.
5. Low Impact Design (LID) measures will be required to be implemented as part of the development storm drainage system.
6. The Conditional Letter of Map Revision (CLOMR) must be approved by FEMA prior to approval of any construction permits.
7. The CC&R's must clearly state that a Landscape Maintenance District (LMD) or a Home Owners Association (HOA) is responsible for maintaining private storm drain infrastructure including any mains, basins, and LID infrastructure.
8. The minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
9. The irrigation diversion structure on the north side of E 5th St must be shown in the improvement plans and referenced in the technical drainage study for the subdivision.
10. The flood conveyance channels necessary for the CLOMR must be built with this subdivision. A sedimentation basin must be constructed as part of these improvements at the 90 degree turn of the Vicee channel.
11. Alexis Avenue must be renamed to make a continuation of Appaloosa.
12. Landscaping plans for the construction permit must include site distance triangles showing that sight distance is not inhibited.
13. Landscaping plans for the construction permit must show distances to existing and proposed water, sewer and storm drain mains to ensure a minimum of 10 foot spacing from trees.
14. A 10 foot wide multiuse pedestrian access path must be provided between the development and the future phase to the south.
15. The following street names cannot be used: Adam St, Maximus Ave, Alexis Ave, and

Dianna St.

16. The utility plans for the construction permit must indicate precast manholes and bases. Cast in place manholes will not be allowed, regardless depth of new sewer mains. Riser depths must meet Carson City Standard Details.
17. A geotechnical report will be required for the subdivision prior to approval of any construction permits.
18. Local roads will have a minimum ACC pavement thickness of 4 inches.

B. Conditions to be Completed Prior to Submitting for Construction Permit or Final Map

19. A FEMA Conditional Letter of Map Revision (CLOMR), reflecting changes due to flood conveyance channels, must be approved by FEMA, prior to submitting plans for a construction permit. Any application submitted prior to FEMA approval of the CLOMR, will not be accepted.
20. All channels and associated access must be shown as separate parcels on the final map to be dedicated to the City.

C. Conditions Related to the Lompa Ranch Phasing Plan

21. The following change must be made to the master phasing plans and reports. The master phasing plans and reports must be included as part of a development agreement for the entire SPA area prior to approval of any construction permit or any tentative map acceptance, whichever comes first.
22. Note that geotechnical investigations will be required to establish design parameters for individual phases.

WATER:

23. The parallel water mains in the phasing plan must be shown as one single water main, however looping must be maintained.
24. The water infrastructure improvements exhibit must be updated to accurately reflect the new facilities required or predecessor to each phase. This column must indicate which mains are required to create looping for each phase. For instance, phase B1 calls for a main to be extended along Robinson Street, however a main would also need to be extended along the Spine Road from 5th Street in order to create a loop.
25. Note that no water main shall have more than 15 services without looping.

SEWER:

26. •The sewer infrastructure improvements exhibit must note that cast in place manholes will not be allowed, regardless depth of new sewer mains, and that riser depths must meet Carson City Standard Details.

STORM DRAIN & FLOOD MANAGEMENT:

27. Plans must show and note that all flood channels (Vicee Canyon, Ash Canyon, and Kings Canyon) must provide sufficient access for City maintenance equipment along the full

length, with access points spaced out no more than every 660 feet, and must note that Robinson St, the Spine Road, E 5th St, and N Saliman Rd are not to be considered part of this access.

28. Note that all flood channels and associated access must be on separate parcels to be dedicated to the City.
29. Note that privately owned and maintained LID/Water Quality facilities are required for each development.
30. The phasing drainage study must demonstrate the ability of downstream drainage facilities to handle increased runoff if detention is not used.
31. Note in the phasing plan that all structures must meet the Flood Protection Ordinance where the lowest floor is two feet above the base flood elevation of the FEMA 1% chance flood or the onsite 1% chance flood whichever is higher.
32. Note in the phasing plan that the minimum clear space between the top edge of the Ash Canyon flood channel and the 24 inch water main is to be 10 feet.
33. Note in the phasing plan that any crossings of flood channels must meet a 100-year flow capacity plus 18 inches of freeboard and must be a clear opening, no multi barrel pipes.
34. Note that drainage studies for all development phases shall demonstrate compliance with Floodplain Storage Capacity Protection requirements of CCMC 12.09.080 (9).
35. Note that drainage studies for all development phases shall provide emergency flow paths for a one hundred (100) year peak storm in accordance with Development Standards.
36. The developer must design the Kings Canyon flood channel such that the drainage and/or any water rights associated with parcels 010-041-34 and 010-041-035 are not adversely affected.

TRAFFIC:

37. Note that Traffic Impact Studies required for all phases must demonstrate that the segment of N Saliman Rd between E William St and E Robinson St will have a projected level of service of C or better for year 2025 unless the North-South Spine Road is connected to William Street. The North-South Spine Road must connect to William Street prior to any development that would cause a level of service worse than C for this segment of road.
38. Note that Traffic Impact Studies required for all phases must demonstrate that the northbound leg and the westbound left turning movement of the N Saliman Rd/E William St intersection and the overall intersection will have a projected level of service of D or better for year 2025 unless the North-South Spine Road is connected to William Street. The North-South Spine Road must connect to E William Street prior to any development that would cause a level of service worse than D for the northbound leg or the westbound left turning movement of this intersection. Also note that traffic impact studies for each phase west of I580 will require traffic counts at this intersection.
39. Note that local roads will have a minimum Asphalt Concrete pavement thickness of 4 inches.
40. Phasing plan maps must be updated to show east-west connectivity between streets in

phase A1 and phase B1, and a 10 foot multiuse path between phase A1 and phase A2.

D. General Conditions

41. Prior to the recordation of the final map for any phase of the project, the improvements associated with said phase must either be constructed and approved by the City, or the specific performance of said work secured by providing the City with a proper surety in the amount of one hundred fifty percent (150 %) of the engineer's estimate. In either case, upon acceptance of the improvements by the City, the developer shall provide the City with a proper surety in the amount of ten percent (10 %) of the engineer's estimate to secure the Developers obligation to repair defects in workmanship and materials which may appear in the work within one year of acceptance by the City

These comments are based on very general plans. All applicable code requirements will apply whether mentioned in this letter or not.

February 13, 2017

Parks, Recreation, and Open Space Department Comments:

1. General Comments

- a. The applicant will enter into a developer agreement with the City. This agreement will include terms and conditions for the funding of the design, construction, and dedication of park, recreation and path facilities within the Lompa Ranch North Specific Plan area. The agreement will outline the City's process for the collection and distribution of Residential Construction Tax (RCT) compliant with CCMC 15.60. The agreement must be considered and approved by the Board of Supervisors prior to recording the Final Map.
- b. The phasing plan will be modified to identify phase triggers and unit counts for park, recreation and path facilities for the Lompa Ranch North Specific Plan area. The phasing plan will be modified and approved by the Board of Supervisors prior to recording the Final Map.
- c. A private Home Owner's Association (HOA), Landscape Maintenance District (LMD), or similar instrument will be established for the Lompa Ranch North Specific Plan area to provide for the operations and maintenance of all park, recreation and path facilities. Operation and maintenance standards for these facilities will be established by the City. The applicant will draft an agreement for the Board of Supervisor's consideration and approval no later than issuance of the certificate of occupancy for the 200th residential unit.
- d. A private Home Owner's Association (HOA), Landscape Maintenance Association (LMA), or similar instrument will be formed to provide 100% funding and maintenance for all the following areas in perpetuity: Common landscape and open space areas, buffer areas between the development and neighborhoods, landscaping associated with the development's path system, landscape medians, street corridors, non-public recreation facilities/amenities, detention basins, and drainage channels. The maintenance and funding shall be addressed in the developer agreement to the satisfaction of the Board of Supervisors. Common area maintenance shall include at a minimum, but not limited to the following:
 - i. Debris, weed, and litter removal
 - ii. Noxious and invasive weed management, including fire prevention
 - iii. Care and replacement of plant material
 - iv. Plant material irrigation and irrigation system repair
- e. As the development's phases are implemented, the plans will be submitted for review by the City. The applicant shall be required to demonstrate pedestrian connectivity between the neighborhood parks, "off-street/paved/shared" multi-use paths, and sidewalks. This shall be done to the satisfaction of the Parks, Recreation, and Open Space Department.
- f. All "off-street/paved/shared" multi-use paths and sidewalks will conform to the standards and policies outlined in of the Carson City Unified Pathways Master Plan adopted April 6, 2006 (as revised March 15, 2007) and as amended in the future. There will be adequate pedestrian

connectivity, throughout the development that provides convenient and logical access to neighborhood parks and paths and enhances the overall sidewalk network within the development.

2. Neighborhood Park Comments (10 acre park / west of I-580)

a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 400th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation and Open Space Department.

b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including water conservation design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.

c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 750th residential unit. Upon successful completion, final project acceptance of said work will be done to the satisfaction of the City, through its Parks, Recreation and Open Space Department.

3. Neighborhood Park Comments (3 acre park / east of I-580)

a. The planning for the park will commence with the completion of a conceptual site plan no later than the issuance of the certificate of occupancy for the 100th residential unit subject to review, approval, and execution of agreed upon terms and conditions memorialized in the developer agreement. The planning process and public meetings shall be coordinated through and agreed upon by the Carson City Parks, Recreation, and Open Space Department.

b. The applicant, at its expense, will design the park. The design will incorporate a universally accessible playground, compliant with the Americans with Disability Act, and be consistent with the department's guidelines and development standards, including water conservation design elements. The design process will be coordinated with the Parks, Recreation, and Open Space Department and include consideration by the Carson City Parks and Recreation Commission.

c. At the applicant's expense, the park will be constructed, accepted, and the land dedicated to the City prior to the issuance of the certificate of occupancy for the 250th residential unit. Upon successful completion, final project acceptance of said work will be done to the satisfaction of the City, through its Parks, Recreation, and Open Space Department.

4. Off-street/paved/shared Path Comments

1. Robinson Street and the Spine Road shall be constructed as full street improvements, to City standards and engineering requirements, including the construction of "on-street bike lanes" and concrete "off-street/shared/paved" multi-use paths. The path along Robinson Street will

be constructed on the road's south side and the path along the Spine Road will be constructed on the road's east side.

2. All multi-use paths will be designed and constructed to a 10' wide (minimum) AASHTO standard concrete multi-use path (off street/paved/shared) with an adjacent 3' wide decomposed granite path.
3. The multi-use paths will include landscaping with a variety of trees (either evergreen or deciduous) that will be planted at a rate of 1 tree per 50 lineal feet (tree groupings are acceptable) with a minimum of 4 shrubs per tree.
4. Path amenities include but are not limited to park benches/seating areas (per 1000 lineal feet of trail along the path), pet waste stations/trash cans, signage depicting direction and trail distance.

Other comments

1. The applicant is required to use best management practices during construction to prevent the spread of noxious and invasive weeds and will incorporate language in construction documents to ensure contractors and subcontractors comply. The Parks, Recreation and Open Space Department will assist the applicant with this condition.

January 24, 2017

TSM-17-005

Fire

1. Project must comply with the currently adopted fire code and applicable amendments adopted by Carson City.
2. Hydrant spacing as shown on Alexis Ave doesn't meet the 2012 IFC Appendix C spacing.
3. The following street names can't be used as they don't comply with CCMC T18 Appendix Division 22: Dianna Street, Adam Street, Alexis Avenue.
4. The cul-de-sac at the end of Achilles Street is too small. It must comply with the 2012 IFC Appendix D figure D103.1
5. The traffic circle mentioned in the traffic study comment 1/response 1 on the single lane traffic circle- the traffic circle must have at least 20' total width for travel lanes.

Dave Ruben
Fire Marshal
Carson City Fire Department
777 S. Stewart Street
Carson City, NV 89701

Direct 775-283-7153
Main 775-887-2210
FAX 775-887-2209

February 8, 2017

Major Project Review Committee

Re: # TSM – 17-005

Dear Kathe,

After initial plan review the Carson City Environmental Control Authority (ECA), a Division of Carson City Public Works Department (CCPW), has the following requirements per the Carson City Municipal Code (CCMC) and the Uniform Plumbing Code (UPC) for the TSM 17-005 (Black Stone) project:

1. Project will need to meet all applicable codes found in Title 12.06 and Appendix 18 Division 15.5 of the Carson City Municipal Code (CCMC) and all applicable codes found in Chapters 7 and 10 of the 2012 Uniform Plumbing Code (UPC).

Please notify Mark Irwin if you have any questions regarding these comments, I can be reached at 775-283-7380.

Sincerely;

Mark Irwin
Environmental Control Officer 3

c: Kelly Hale, Environmental Control Supervisor

BRIAN SANDOVAL
Governor

STATE OF NEVADA

KAY SCHERER
Interim Director

JASON KING, P.E.
State Engineer



DEPARTMENT OF CONSERVATION AND NATURAL RESOURCES
DIVISION OF WATER RESOURCES

901 South Stewart Street, Suite 2002
Carson City, Nevada 89701-5250
(775) 684-2800 • Fax (775) 684-2811
<http://water.nv.gov>

February 13, 2017

Tentative Subdivision Review No. 20897-T

RE: Permit None

To: Hope Sullivan
Carson City Planning Division
108 East Proctor Street
Carson City, NV 89701

Name: **Blackstone Ranch – Phase 1**

County: Carson City– Fifth Street and North Saliman Road

Location: A portion of Section 16, Township 15 North, Range 20, East, MDB&M.

Plat: Final: One-hundred-eighty-nine (189) lots, common areas, and right-of-ways totaling approximately 44.55 acres and being Carson City Assessor's Parcel Number 10-041-70.

**Water Service
Commitment**

Allocation: No water has been committed at this time.

**Owner-
Developer:** Thomas B. & Martha L. Keating Family Trust dated April 5, 1995
161 Plantation Drive
Carson City, NV 89703

Engineer: The Red Ltd.
7272 South El Capitan Way, Suite 2
Las Vegas, NV 89148

**Water
Supply:** Carson City Public Works

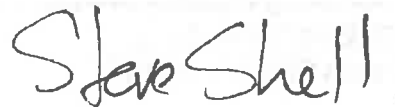
General: A tentative subdivision map was presented and approved by this office on February 13, 2017 as described on the *Blackstone Ranch – Phase 1* map.

Municipal water service is subject to Carson City Public Works rules and regulations and approval by the Office of the State Engineer regarding water quantity and availability.

A Will Serve from Carson City Public Works and final mylar map of the proposed project must be presented to the State Engineer for approval and signed through his office prior to development.

Action: Tentative approval of *Blackstone Ranch – Phase 1* subdivision based on acceptance of Water Will Serve by Carson City Public Works.

Best regards,

A handwritten signature in black ink that reads "Steve Shell". The signature is written in a cursive, slightly slanted style.

Steve Shell
Water Resource Specialist II

SLS/sls



Carson City School District

1402 West King Street, Carson City NV 89703

(775) 283-2000 - Fax: (775) 283-2090

February 9, 2017

Hope Sullivan
Carson City Planning Department
108 E Proctor Street
Carson City, NV 89701

RE: Lompa Project – School District Impact

Ms. Sullivan,

The purpose of this communication is to inform you of a very productive meeting that took place on January 31, 2017, between the developers of the Lompa property here in Carson City and the Carson City School District. In attendance were:

Josh Myers, President, Blackstone Development
Mike Railey, Rubicon Design Group
Loren Chilson, TrafficWorks

Richard Stokes, Superintendent, Carson City School District
Mark Johnson, Capital Project Manager, Carson City School District
Mark Korinek, Director of Operations, Carson City School District
Andrew Feuling, Director of Fiscal Services, Carson City School District

The meeting was scheduled so that the District could gain a better understanding of the project as it moves forward and the impact it may have on our operations, finances and planning in that area.

Blackstone has reserved a 10-acre parcel of land, adjacent to the current location of Carson High School, on Robinson Street, for the district to construct a new school. Based on our own internal forecasting, this would most likely be a new elementary school. Our understanding is that this land would become available to the District when a threshold of 700 units have been sold in the development, which is estimated to take 4-5 years.

We also learned about the estimated impact the Lompa development would have on Robinson Street, which would become a through-street, as well as the desire to create a new road out of this subdivision that would terminate on Highway 50 near Gold Dust West Casino. Also, the intersection of Saliman and Robinson would warrant a traffic signal when the AM peak hour bi-

directional traffic volume on Robinson Street, just east of Saliman Road, exceeds 600 total vehicles.

Blackstone offered to send a copy of the phasing plan to us so that we can better predict the impact on the district over time and we look forward to seeing the document. They also agreed to send representation to our Facilities Master Plan Committee meetings so they better understand our greater concerns of growth throughout the district and to hear the concerns of our community members that attend the meetings because of their interest in the schools. We are excited to embrace them there as a partner in the future of Carson City and the Carson City School District.

We do have some concerns that we would like to bring to your attention as your discussions continue, in various arenas, related to the development.

1. Traffic impact on Robinson Street

- a. Regardless of the outcome of proposed new road exiting near Gold Dust West Casino, we feel that the traffic study likely is underestimating the traffic density that will come with the development. We believe it is underestimated because of the limited road access to the development area and because the "peak hours" considered in the study did not take into account the dismissal time for Carson High, let alone the expected impact of dismissal at an adjacent new school on Robinson Street.
- b. The increased students coming from the development would also increase the number of buses and bus trips coming out of our District bus garage, which is located on Robinson, behind Carson High School.
- c. This underestimation would lead to great safety concerns for both students and staff in the area, especially for students walking to and from school.
- d. There would also be safety concerns for bus loading on the street.
 - i. We believe that the two suggested cross sections would not meet our needs.
 - ii. We could move the bus loading off of the street, but we have no incentive to consider that as we would incur substantial costs to construct an appropriate area off-street and we are happy with the current bus loading location and process.

2. New school – land availability and cost

- a. The 10-acre parcel reserved for the school district would not become available until the 700th unit is sold. While we have more calculations to do on our side, but because it takes two years to build a school, we feel that may be a little late for our families' needs. We are currently near capacity at all of our schools and while we are certainly excited to see Carson City grow, this could pose a serious problem.
- b. We believe the location adjacent to Carson High School to be a good one, we are continuing internal discussions to consider the wider impact.
- c. Revenues to offset construction costs

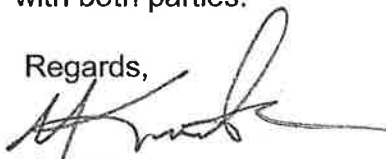
- i. Carson City's population is just over 55,000, which precludes us from considering to levy a Residential Construction Tax to help offset the costs to the District of community growth.
- ii. School Districts in the State of Nevada cannot collect Impact Fees to offset the costs of growth.
- iii. We cannot levy a Washoe-styled new sales tax to fund the construction of capital items.
- iv. We cannot collect property taxes for homes that don't yet exist. Collecting property taxes on 700 new homes (when the land is proposed to become available to us) would not generate the bonding capacity to build a new \$25,000,000 elementary school, let alone pay for the land to put under it. We are fully bonded out and with what appears to be an environment of increasing interest rates, we are very concerned.
- v. Carson City was able to leverage a Condition of Development to provide a park and fire station as part of the development but the District does not have this leveraging authority nor ability.

3. Drainage

- a. Carson High School currently has problems with a drainage ditch on the northeast side of our property which will also become a concern for the Lompa development in that area.
- b. Mr. Myers offered to meet with our staff and the Carson City Storm Water Chief in that location to discuss mutually beneficial options, which we greatly appreciate.
- c. CCSD raised the suggestion of piping and burying the drainage ditch on CHS campus due to major safety concerns this year. Mr. Myers mentioned that, "all of our drainage concerns will be taken care of".

Again, this was a great meeting and we are excited to be partners with Blackstone and Carson City in this endeavor. We did express these concerns to Blackstone and wanted to relay them to you as well so that all three of us can work together to find solutions or better understandings. We look forward to learning more and being involved in future conversations with both parties.

Regards,



Mark Korinek
Director of Operations

TSM-17-005

From: Paul Weaver <pnlweaver@sbcglobal.net>
Sent: Tuesday, February 14, 2017 9:59 PM
To: Planning Department
Subject: Comments for Planning Commission
Attachments: subdivision letter.docx



See attached comments relative to the proposed subdivision on Saliman to be discussed 2/22.

Thanks,
Paul Weaver

Feb 14, 2017

Dear Planning Commission,

Thank you for notifying us of the potential subdivision to be built near us. We are pleased that it will be a tract of single family homes and not multifamily dwellings.

By looking at the tiny plan you provided it seems that little thought was given to traffic flow. For all of those 189 houses only 2 roads lead out of the subdivision and only one of those two connectors is longer than half a block. The connection to Saliman will back up to other streets in the subdivision. Saliman is a busy street and is sometimes difficult to enter. My wife & I live on Appaloosa Ct. This will further complicate our turns onto Saliman. Appaloosa is a cul de sac and doesn't go through. It would make more sense to enter & leave from a street that goes through. Perhaps Goldfield. Or perhaps it would make more sense to make this subdivision seem like part of Carson City by having all of the streets line up with the other streets coming into Saliman and have actual intersections. That way each street would have less traffic.

The other entrance to the subdivision dumps out on Robinson right near the high school. That street, twice a day, is a disaster because of the buses and other traffic generated by the school. There is no alternative to going through all that traffic, both cars and pedestrian. This congestion occurs on both Robinson and Saliman. There should be a third way in & out of the subdivision. 5th street would be the obvious choice and the vicinity map says it should be possible.

The plan doesn't indicate what traffic controls would be in place to handle this added traffic. Both Appaloosa and Robinson will need help for all of our safety and the safety of the high school students who walk and ride on both Saliman and Robinson..

Another timely issue is that of flood control. How will the excessive amounts of water that flow through this area be handled? In this last storm Saliman carried water uverflowing from both Mills park and the trench along 5th street.

We trust that this plan is only a starting point and will receive much more scrutiny before it is built.

Thank You,
Paul and Lynne Weaver 1501 Appaloosa Ct, CC,NV

Carson City Planning Division
108 E. Proctor Street· Carson City NV 89701
Phone: (775) 887-2180 • E-mail: planning@carson.org

FOR OFFICE USE ONLY:

TENTATIVE MAP FOR A SUBDIVISION

FILE # TSM - ~~10~~ - TSM - 17 - 005

APPLICANT PHONE #
Blackstone Development Group, Inc 775-352-4200

STATE FEES: See checklist. Submit the two state checks at the time of initial application submittal.

MAILING ADDRESS, CITY, STATE, ZIP
439 W. Plumb Ln., Reno, NV 89509

FEE: \$3,500.00+ noticing fee + CD containing all application data (to be submitted when application is deemed complete by staff)

ENGINEER PHONE #
Rubicon Design Group, LLC 775-425-4800

SUBMITTAL PACKET

See checklist (fill out checklist and return to staff with the application packet)

MAILING ADDRESS, CITY, STATE, ZIP
100 California Ave. Suite 202, Reno, NV 89509

EMAIL ADDRESS
mrailey@rubicondesigngroup.com

Application Reviewed and Received By:

RECEIVED

PROPERTY ADDRESS, CITY, STATE, ZIP
East side of Saliman Rd., south of Robinson Street, north of 5th St., Carson City, NV 89701

JAN 13 2017

PRESENT ZONING APN(S)
SF-6 010-041-70

CARSON CITY
PLANNING DIVISION

REQUEST: In accordance with the provisions of Title 17 of the Carson City Municipal Code, application is hereby made for a Planned Unit Development on property situated at:

The required modifications to Carson City's Land Use Regulations are as follows:
No modifications to code are being requested. The proposed map conforms to the standards of the Carson City Municipal Code, as well as the specific requirements of the Lompa Ranch North Specific Plan Area. Please refer to attached report, plans and professional reports for a detailed project description.

ACKNOWLEDGMENT OF APPLICANT: (a) I certify that the foregoing statement are true and correct to the best of my knowledge and belief; (b) I agree to fulfill all conditions established by the Board of Supervisors.

Thomas B. Keating Martha L. Keating
Applicant's Signature

Jan 10, 2017
Date

PROPERTY OWNER'S AFFIDAVIT

Thomas B. Keating Martha L. Keating, being duly deposed, do hereby affirm that I am the record owner of the subject property, and that I have knowledge of, and I agree to, the filing of this application.

Martha L. Keating
Signature *Thomas B. Keating*

141 Plantation Ave
Address

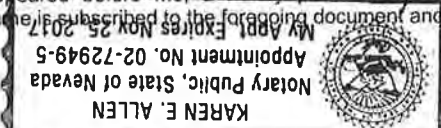
1-10-17
Date

Use additional page(s) if necessary for other names.

STATE OF NEVADA *Douglas*
COUNTY *January 10, 2017*

On *Douglas*, 2017, personally appeared *Thomas B. Keating and Martha L. Keating*, personally known (or proved) to me to be the person whose name is subscribed to the foregoing document and who acknowledged to me that he/she executed the foregoing document.

Karen E. Allen
Notary Public



NOTE: In order to avoid unnecessary time delays in processing your develop project, it is important that it be as complete as possible when submitted. A checklist is available to assist you and your engineer. If you have further questions regarding your application, please call the Planning Division at 775-887-2180.



BLACKSTONE RANCH - PHASE 1

Tentative Subdivision Map Application



Photo Credit: aroundcarson.com

Prepared by:



January 12, 2017



TSM - 17 - 005

BLACKSTONE RANCH – PHASE 1

TENTATIVE SUBDIVISION MAP APPLICATION

Prepared for:

Blackstone Development Group, Inc.

439 W. Plumb Lane

Reno, Nevada 89509

Prepared by:

Rubicon Design Group, LLC

100 California Avenue, Suite 202

Reno, Nevada 89509

(775) 425-4800

January 12, 2017



BLACKSTONE RANCH - PHASE 1

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Project Description	5
Master Plan Policy Checklist	8
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Figure 3 – Master Plan Land Use	3
Figure 4 – Zoning Map	4
Figure 5 – Preliminary Site Plan	6

Attachments:

- Carson City Application Form and Checklist
- Preliminary Engineering Plans
- Preliminary Landscape Plans
- Drainage Report (Master Drainage Report and Conceptual Drainage Study)
- Traffic Impact Analysis (Overall Lompa Ranch and Phase 1)
- Aquatic Resources Delineation Report
- Preliminary Geotechnical Report (including addendum)
- T&E Species Evaluation Memo
- Blackstone Ranch Utility Analysis (Water and Sewer)



BLACKSTONE RANCH - PHASE 1

Introduction

This application includes the following request:

- A **Tentative Subdivision Map Application** for the consideration of a 189-unit single family residential subdivision within the Lompa Ranch North Specific Plan.

Project Location

Blackstone Ranch Phase 1 is located within the Lompa Ranch North Specific Plan area which encompasses 203± acres located on the west side of Interstate 580, north of East Fifth Street, east of Saliman Road, and south of US Highway 50 (East William Street). The remaining 48.04± acres are located on the east side of Interstate 580 along the western side of Airport Road. Specifically, Phase 1 is located east of Saliman Road, south of Robinson Street on the west side of Lompa Ranch. Phase 1 will include 44.5± acres and is located within a portion of an existing 62± acre parcel (APN# 010-041-70). The property lies south of Robinson Street, east of Saliman Road, as depicted in Figure 1 (below).



Figure 1 – Vicinity Map





BLACKSTONE RANCH - PHASE 1

Existing Conditions

Currently, the project site is vacant and consists primarily of pasture land. Figure 2 (below) depicts the existing onsite conditions.

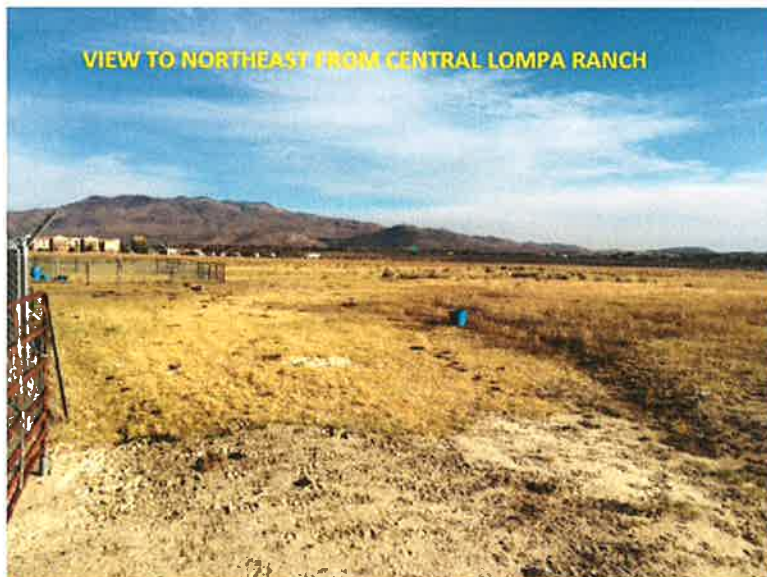
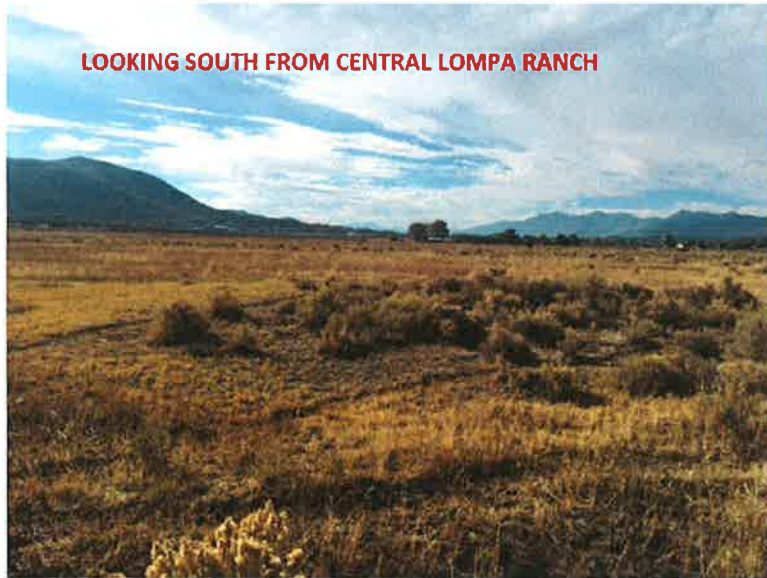


Figure 2 – Existing Conditions



BLACKSTONE RANCH - PHASE 1

As noted previously, the subject property is located within the Lompa Ranch North Specific Plan (SPA). The SPA designates the Blackstone Ranch Phase 1 site as Medium Density Residential (Master Plan) and SF-6 (zoning). Figure 3 (below) depicts the Master Plan land use, while Figure 4 (following page) depicts the adopted zoning.

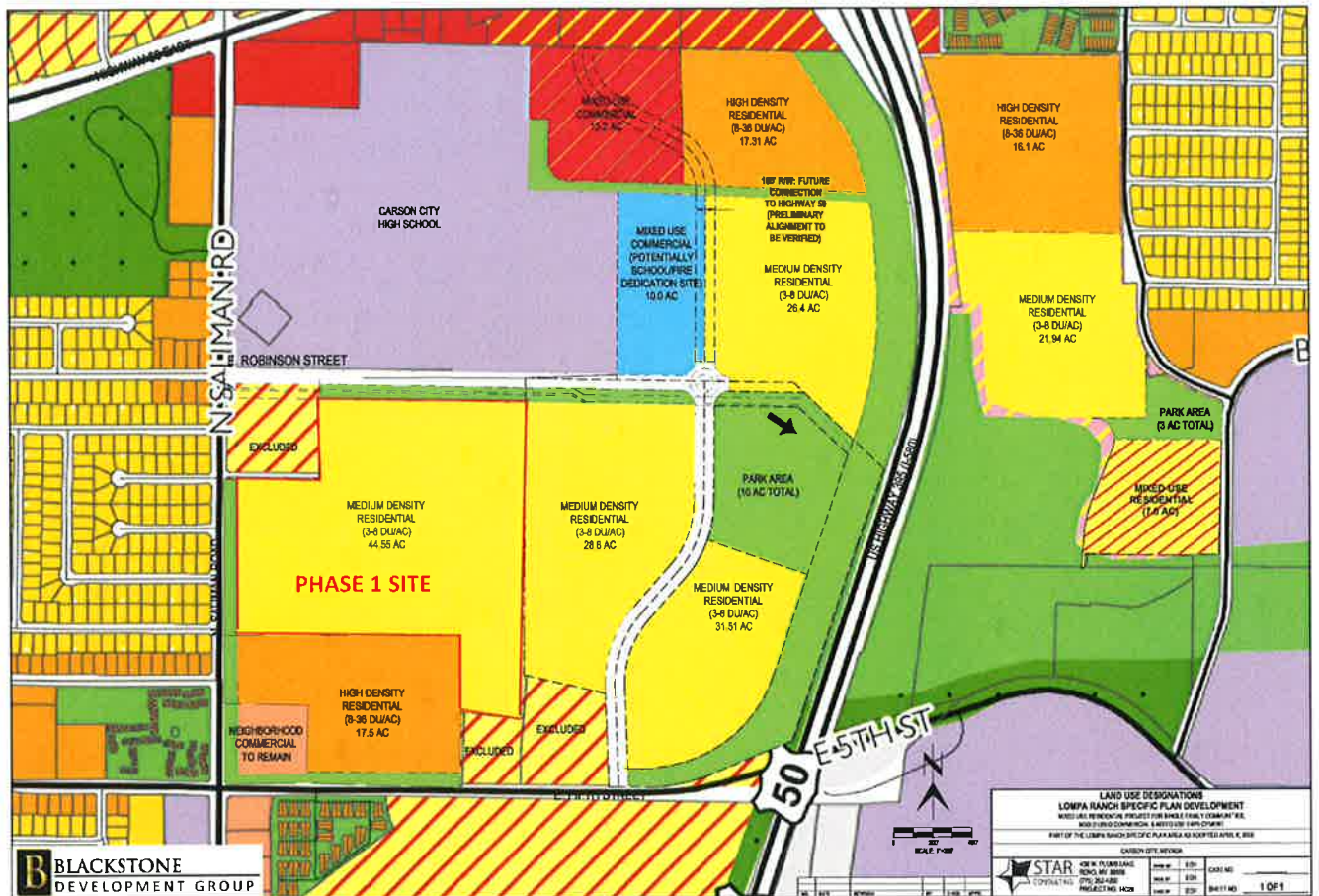


Figure 3 – Master Plan Land Use



BLACKSTONE RANCH - PHASE 1

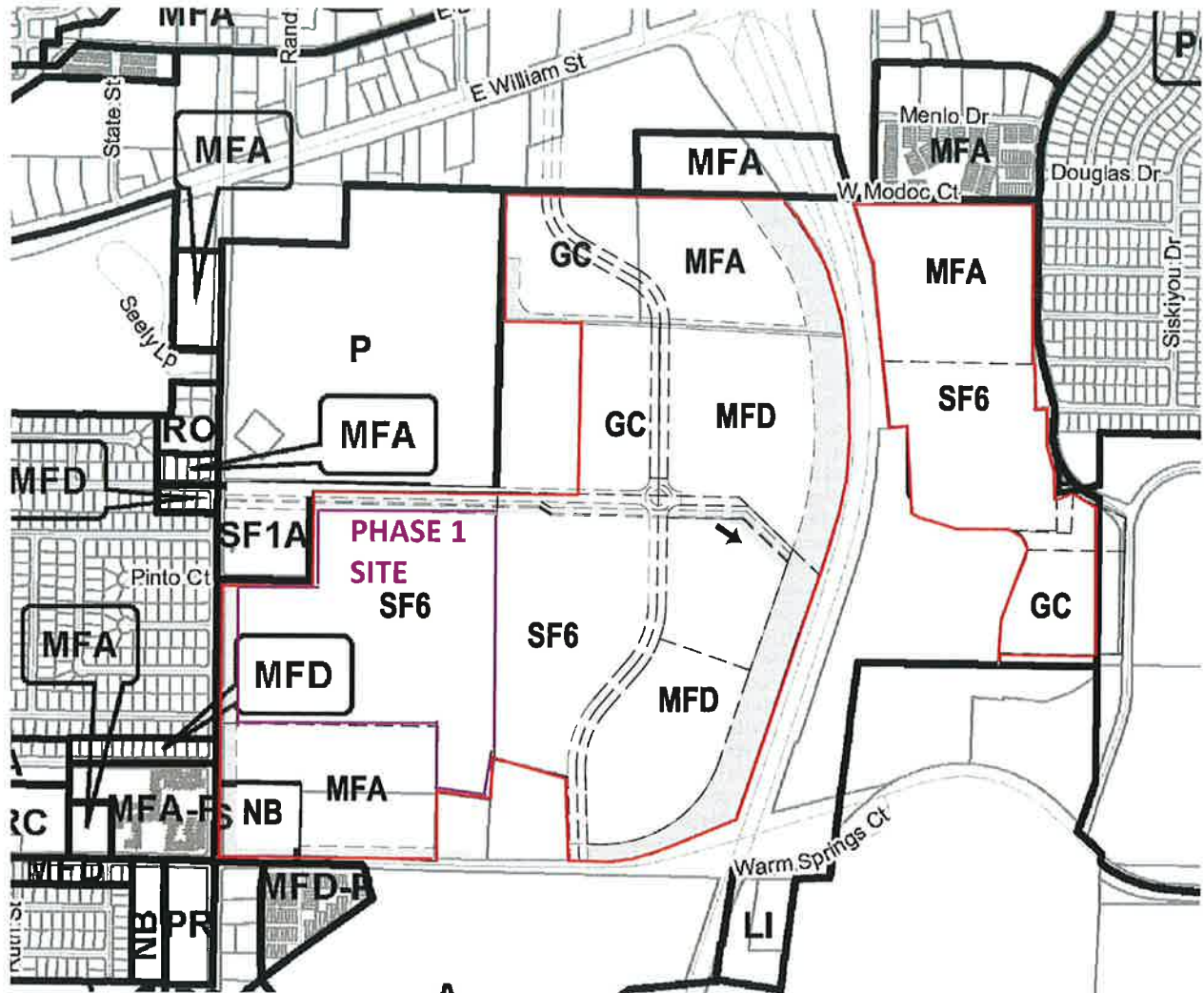


Figure 4 – Zoning Map



BLACKSTONE RANCH - PHASE 1

Surrounding land use includes vacant land to the south and east, Carson High School to the north, single family to the west and a church to the northwest (southeast corner of Saliman and Robinson). Access to the Phase 1 property can be achieved from Saliman Road on the west and Robinson Street at the north.

Project Description

This Tentative Subdivision Map application is for the first phase of the overall Lompa Ranch project. The plan incorporates comments received from Carson City during the Conceptual Map phase of the project review.

Blackstone Ranch Phase 1 is proposed for 189 single family units. Primary access to the development will be via two entries; one located along Saliman Road and one located along Robinson Street. Both accesses will include a "split" entry which will allow for an entry monument that identifies the community, as called out in the Lompa Ranch North Specific Plan.

Consistent with the existing SF-6 zoning, lot sizes will range from 6,005± square feet to 13,1723± square feet, with an overall average lot size of 6,799± square feet. At this time, final home plans (including elevations and floor plans) have not been completed. However, building envelopes are shown on the Tentative Map. Elevations must comply with the standards included within the Specific Plan. This includes the use of varied materials and a minimum of three different elevation options for each model. Additionally, "staggered" setbacks are required to ensure that a monotonous streetscape does not occur.

The Carson City Municipal Code requires that a minimum of 150 square feet of open space area be provided for each individual unit. Based on 189 units, a total of 28,350 square feet of open space is required. As proposed, a total of 114,357± square feet of open space is provided. A homeowners' association along with covenants, conditions and restrictions (CC&R's) will be created for the project and will be responsible for the maintenance of all open space/common areas.

The following table provides an overall summary of Blackstone Ranch - Phase 1:

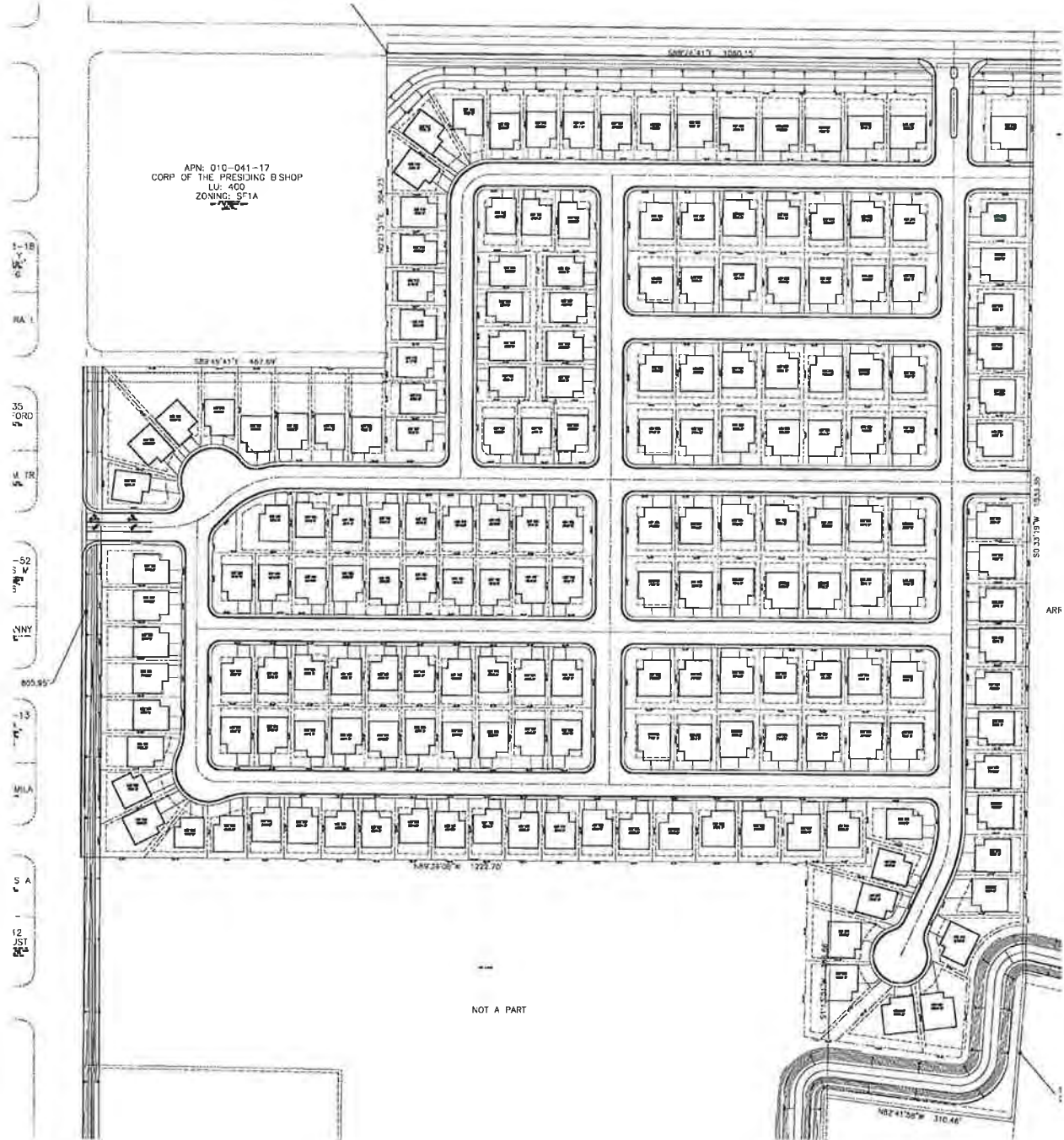
Development Standard	Proposed with Blackstone Ranch – Phase 1
Total Project Area	44.5± acres ¹
Total Units	189
Total Lot Area	31.1± acres
Right-of-Way Area	2.6± acres
Common Area/Open Space	10.8± acres
Project Density	4.25 dwelling units per acre
Minimum Lot Size	6,005± square feet
Maximum Lot Size	13,723± square feet
Average Lot Size	6,799± square feet

1 – Current parcel acreage = 62± acres



BLACKSTONE RANCH - PHASE 1

Figure 5 (below) depicts the preliminary site plan developed for Blackstone Ranch Phase 1.



Note: Refer to attached map packet for full-size plans.

Figure 5 – Preliminary Site Plan



BLACKSTONE RANCH – PHASE 1

Consistent with the Lompa Ranch Specific Plan standards, a pedestrian trail will be constructed along the Robinson Street frontage, adjacent to the proposed open channel. The trail will be paved and constructed to the Unified Pathways Master Plan standards.

In terms of impacts, Blackstone Ranch Phase 1 is compatible with the surrounding area and will not unduly burden existing public services and infrastructure. A comprehensive traffic impact analysis completed by Traffic Works is attached. Based on typical Institute of Transportation Engineers (ITE) estimates, Blackstone Ranch Phase 1 will generate approximately 1,818 average daily trips (ADT) with 141 am peak hour trips and 186 pm peak hour trips. The traffic impact analysis describes all necessary mitigation measures and/or improvements that will be made to ensure appropriate levels of service are maintained.

Blackstone Development Group, Inc., the Master Developer, is currently working with Carson City and the Federal Emergency Management Agency (FEMA) on a Letter of Map Revision (LOMR) to remove the majority of the 251 acres from the floodway and a Conditional Letter of Map Revision (CLOMR) to remove the property from the floodplain. A master drainage analysis is attached and includes a network of storm water channels and improvements that will remove the subject property from the FEMA flood plain. Based on pre-application meetings with Carson City staff, it was determined that tentative maps can move forward and will simply include a condition that requires the developer to demonstrate that the property has been removed from the hazard zone(s). The CLOMR/LOMR process will accomplish this. Also, a preliminary drainage report was completed with the Lompa Ranch North Specific Plan. A more refined report that specifically addresses Blackstone Ranch Phase 1 is included as an attachment to this report.

With only 189 units, overall project impacts will be minimal. The project, as proposed, is consistent with the adopted zoning and Specific Plan standards. The project will be required to pay all applicable impact and City fees, including the adopted fire impact fee included within the Lompa Ranch Specific Plan. Park tax will not be required as the Lompa Ranch Master Developer will be required to construct a 10-acre community park with construction of the 750th unit.

As an infill development, emergency services are already occurring within the area. The project is not anticipated to unduly impact existing levels of service and is in fact providing fire mitigation funds that can help support the construction of an additional fire station in the future.



BLACKSTONE RANCH – PHASE 1

Master Plan Policy Checklist

Consistent with Carson City Tentative Subdivision Map application requirements, this section is taken directly from Carson City documents and forms part of the *Tentative Map* application process. Responses to the checklist questions are included in this section and are printed in **bold type**.

PURPOSE

The purpose of a development checklist is to provide a list of questions that address whether a development proposal is in conformance with the goals and objectives of the 2006 Carson City Master Plan that are related to Master Plan Map Amendments and Zoning Map Amendments. This checklist is designed for developers, staff, and decision-makers and is intended to be used as a guide only.

Development Name:

Reviewed By:

Date of Review:

DEVELOPMENT CHECKLIST

The following five themes are those themes that appear in the Carson City Master Plan and which reflect the community's vision at a broad policy level. Each theme looks at how a proposed Master Plan or Zoning Map Amendment can help achieve the goals of the Carson City Master Plan. A check mark indicates that the proposed amendment meets the applicable Master Plan policy. The Policy Number is indicated at the end of each policy statement summary. Refer to the Comprehensive Master Plan for complete policy language.

CHAPTER 3: A BALANCED LAND USE PATTERN

The Carson City Master Plan seeks to establish a balance of land uses within the community by providing employment opportunities, a diverse choice of housing, recreational opportunities, and retail services.

Is or does the proposed amendment:

- ✓ Consistent with the Master Plan Land Use Map in location and density?

As proposed, Blackstone Ranch Phase 1 is in direct compliance with the existing Medium Density Residential Master Plan designation and SF6 zoning. Additionally, the project is in full compliance with the standards and requirements included within the Lompa Ranch North Specific Plan.

- ✓ Meet the provisions of the Growth Management Ordinance (1.1d, Municipal Code 18.12)?

This project meets the provisions of the Growth Management Ordinance by locating housing in an area that is adjacent to existing roadways and services. The project is an infill development and serves to better maximize the use of Carson City's infrastructure. Infill residential is encouraged within the Master Plan. The project has convenient access to all community services and is appealing to a wide range of potential residents.



BLACKSTONE RANCH - PHASE 1

- ✓ Encourage the use of sustainable building materials and construction techniques to promote water and energy conservation (1.1e and f)?

New development must comply with the standards included within the Lompa Ranch North Specific Plan which include energy efficient building materials as well as locating building envelopes with solar orientation in mind (to the extent possible).

Located in a priority infill development area (1.2a)?

The project site is not in a priority infill area but it is an infill project.

- ✓ Provide pathway connections and easements consistent with the adopted Unified Pathways Master Plan and maintain access to adjacent public lands (1.4a)?

The overall Lompa Ranch project will provide a comprehensive trail network. As such, Phase 1 will include the first links to the ultimate network in accordance with the Specific Plan standards.

- ✓ Encourage cluster development techniques, particularly at the urban interface with surrounding public lands, as appropriate, and protect distinctive site features (1.4b and c, 3.2a)?

The project clusters development and retains significant open space. This open space then serves as an access point to trails and undeveloped areas and exceeds the required minimum by over 3 acres.

At adjacent county boundaries, coordinated with adjacent existing or planned development with regards to compatibility, access, and amenities (1.5a)?

The site is not located along a county boundary.

- ✓ Located to be adequately served by City services including fire and sheriff services, and coordinated with the School District to ensure the adequate provision of schools (1.5d)?

As an infill parcel, the site is bordered by existing development and is within existing service boundaries. City and area services are already occurring within the area and can be provided to this site as well. Also, the project is subject to fire impact fees as adopted in the Lompa Ranch Specific Plan.

In identified Mixed-Use areas, promote mixed-use development patterns as appropriate for the surrounding context consistent with the land use descriptions of the applicable Mixed-Use designation, and meet the intent of the Mixed-Use Evaluation Criteria (2.1b, 2.2b, 2.3b, Land Use Districts, Appendix C)?

The site is not within an identified mixed-use area. However, the overall Lompa Ranch project will be a highly integrated mixed use development. This is simply the first phase in a much larger overall development.



BLACKSTONE RANCH - PHASE 1

✓ Provide a variety of housing models and densities within the urbanized area appropriate to the development size, location and surrounding neighborhood context (2.2a, 9.1a)?

The project will provide new housing options in east Carson City and serves to fill a defined demand for new homes in the area. New homes will incorporate design standards from the Lompa Ranch North Specific Plan and overall density/lot size is consistent with existing single family uses to the west.

Protect environmentally sensitive areas through proper setbacks, dedication, or other mechanisms (3.1b)?

There are no environmentally sensitive areas on the site. A threatened and endangered species evaluation memo is included as an attachment to this report.

If at the urban interface, provide multiple access points, maintain defensible space (for fires) and are constructed of fire resistant materials 3.3b)?

The site is not within an urban/wildlife interface area.

Site outside the primary floodplain and away from geologic hazard areas or follow the required setbacks or other mitigation measures (3.3d, e)?

As noted previously, a CLOMR/LOMR process is currently underway which will remove the subject site from the FEMA flood hazard area(s). A condition will be placed on the tentative map that requires completion of this process prior to construction.

✓ Provide for levels of services (i.e. water, sewer, road improvements, sidewalks, etc) consistent with the Land Use designation and adequate for the proposed development (Land Use table descriptions)?

The project proposes to provide levels of service consistent with what is seen in the area now. As an infill site, it is possible to coordinate the project design with development that adjoins the site. Roads, sidewalks, and utilities will therefore be commensurate with what the neighborhood enjoys now. Trail connections and open space will be improved.

If located within an identified Specific Plan Area (SPA), meet the applicable policies of that SPA (Land Use Map, Chapter 8)?

The project, as proposed, is in full compliance with the Lompa Ranch North Specific Plan.



BLACKSTONE RANCH - PHASE 1

CHAPTER 4: EQUITABLE DISTRIBUTION OF RECREATIONAL OPPORTUNITIES

The Carson City Master Plan seeks to continue providing a diverse range of park and recreational opportunities to include facilities and programming for all ages and varying interests to serve both existing and future neighborhoods.

Is or does the proposed amendment:

- ✓ Provide park facilities commensurate with the demand created and consistent with the City's adopted standards (4.1b)?

The project will provide substantial open space area that will benefit the neighborhood. The project is therefore proposing amenities well above what is required by Code and by normal planning practice. Also, as the Lompa Ranch master plan develops, a new community park and trail network will be provided per the Lompa Ranch North Specific Plan.

- ✓ Consistent with the Open Space Master Plan and Carson River Master Plan (4.3a)?

This project advances the goals of the Open Space Master Plan through its use of an infill site and through the provision of park/open space area. The project does not extend development into wildland areas.

CHAPTER 5: ECONOMIC VITALITY

The Carson City Master Plan seeks to maintain its strong diversified economic base by promoting principles which focus on retaining and enhancing the strong employment base, include a broader range of retail services in targeted areas, and include the roles of technology, tourism, recreational amenities, and other economic strengths vital to a successful community.

Is or does the proposed amendment:

- ✓ Incorporating public facilities and amenities that will improve residents' quality of life (5.5e)?

As detailed above, the project will provide public amenities in the form of park space and enhanced trails.

Promote revitalization of the Downtown core (5.6a)?

Not applicable.

Incorporate additional housing in and around the Downtown, including lofts, condominiums, duplexes, live-work units (5.6c)?

Not applicable.



BLACKSTONE RANCH - PHASE 1

CHAPTER 6: LIVABLE NEIGHBORHOODS AND ACTIVITY CENTERS

The Carson City Master Plan seeks to promote safe, attractive and diverse neighborhoods, compact mixed-use activity centers, and a vibrant, pedestrian-friendly Downtown.

Is or does the proposed amendment:

- ✓ Provide variety and visual interest through the incorporation of varied lot sizes, building styles and colors, garage orientation and other features (6.1b)?

As required per the Specific Plan, new homes will be required to provide a mix of building materials in order to provide for more diverse architecture. This, coupled with staggered setbacks will ensure a visually appealing streetscape. Also, all floor plans will be required to have a minimum of 3 distinct elevations. This ensures that the neighborhood has visual interest and that all of the homes will not look alike.

- ✓ Provide variety and visual interest through the incorporation of well-articulated building facades, clearly identified entrances and pedestrian connections, landscaping and other features consistent with the Development Standards (6.1c)?

The Lompa Ranch North Specific Plan standards far exceed the requirements of the Carson City Municipal Code. This ensures that there will be enhanced landscaping, distinctive entry monuments, upscale architecture, etc.

- ✓ Provide appropriate height, density and setback transitions and connectivity to surrounding development to ensure compatibility with surrounding development for infill projects or adjacent to existing rural neighborhoods (6.2a, 9.3b 9.4a)?

The project will be complementary to surrounding development in terms of height, setbacks, and use and will therefore be directly compatible.

If located in an identified Mixed-Use Activity Center area, contain the appropriate mix, size and density of land uses consistent with the Mixed-Use district policies (7.1a, b)?

The project is not in a mixed-use activity center.

If located Downtown:

- o Integrate an appropriate mix and density of uses (8.1a, e)?

Not applicable.



BLACKSTONE RANCH – PHASE 1

o Include buildings at the appropriate scale for the applicable Downtown Character Area (8.1b)?

The project is not located downtown.

o Incorporate appropriate public spaces, plazas and other amenities (8.1d)?

The project is not located downtown however it does include public spaces.

CHAPTER 7: A CONNECTED CITY

The Carson City Master Plan seeks promote a sense of community by linking its many neighborhoods, employment areas, activity centers, parks, recreational amenities and schools with an extensive system of interconnected roadways, multi-use pathways, bicycle facilities, and sidewalks.

Is or does the proposed amendment:

✓ Promote transit-supportive development patterns (e.g. mixed-use, pedestrian-oriented, higher density) along major travel corridors to facilitate future transit (11.2b)?

The project is located along existing streets and is within walking distance of schools and commercial uses. Also, the site is within walking distance of existing transit stops.

✓ Maintain and enhance roadway connections and networks consistent with the Transportation Master Plan (11.2c)?

The project is accessed by the existing roadway network. It will also fill some existing gaps in the roadway network by providing additional improvements as depicted on the attached plans.

✓ Provide appropriate pathways through the development and to surrounding lands, including parks and public lands, consistent with the Unified Pathways Master Plan and the proposed use and density (12.1a, c)?

The project will provide for a pedestrian path as called out in the Specific Plan, consistent with the Unified Pathways Master Plan.



BLACKSTONE RANCH - PHASE 1

Tentative Map Findings

Section 17.07.005 of the Carson City Municipal Code establishes findings that the Planning Commission and/or Board of Supervisors must make in approving a tentative subdivision map. These findings are listed below and are addressed in **bold face** type.

In considering parcel maps, planned unit developments and tentative subdivision maps the director shall consider the following:

1. Environmental and health laws and regulations concerning water and air pollution, the disposal of solid waste, facilities to supply water, community or public sewage disposal and, where applicable, individual systems for sewage disposal.

Blackstone Ranch Phase 1 serves as an infill project within an established area of the City. Therefore, all necessary infrastructure and municipal services necessary to serve the project are in place or can easily be extended (at the expense of the developer). The project will be served by municipal water and sewer, solid waste disposal, NV Energy, Southwest Gas, cable television, etc. in accordance with Carson City and State of Nevada standards.

2. The availability of water which meets applicable health standards and is sufficient in quantity for the reasonably foreseeable needs of the subdivision.

Blackstone Ranch will be served by the existing municipal water system and it will be demonstrated by the project applicant that sufficient water rights have been dedicated/acquired to serve the project.

3. The availability and accessibility of utilities.

As an infill development, all necessary utilities are in place or can be easily extended to serve the project.

4. The availability and accessibility of public services such as schools, police protection, transportation, recreation and parks.

The project is located within the developed core of the City and is therefore located in an area where all City services and infrastructure exist. Additionally, as part of the overall Lompa Ranch SPA, new schools, parks, and community amenities are planned and mandated through the SPA.

5. Access to public lands. Any proposed subdivision that is adjacent to public lands shall incorporate public access to those lands or provide an acceptable alternative.

Not applicable. However, the project does provide pedestrian trail links per the requirements of the Lompa Ranch SPA.



BLACKSTONE RANCH - PHASE 1

6. Conformity with the zoning ordinance and land use element of the city's master plan.

The project is in direct compliance with the existing Master Plan designation. Furthermore, the project complies with the SF-6 zoning in terms of overall density and character.

7. General conformity with the city's master plan for streets and highways.

With only 189 units, the project will be adequately served by the existing roadway network and will result in negligible impacts. A comprehensive traffic impact analysis is attached and provides specific details and mitigation measures.

8. The effect of the proposed subdivision on existing public streets and the need for new streets or highways to serve the subdivision.

As outlined in the attached traffic impact analysis, all impacts associated with Blackstone Ranch Phase 1 can easily be mitigated and no significant impacts are anticipated.

9. The physical characteristics of the land such as flood plains, earthquake faults, slope and soil.

The project is well suited for the type of development proposed, especially given the planned drainage improvements (as noted in the attached drainage report). The project site contains no faults or unusual soils. Attached to this report are detailed engineering plans, reports, and analyses that provide further details. The site is in an infill area and is part of the developed core of Carson City.

10. The recommendations and comments of those entities reviewing the subdivision request pursuant to NRS 278.330 thru 278.348, inclusive.

This application package will be sent to reviewing agencies per the requirements of the Carson City Municipal Code and Nevada Revised Statutes. Once comments are received, they can be incorporated into the final design of the project or included as conditions of approval of this tentative subdivision map request.

11. The availability and accessibility of fire protection including, but not limited to, the availability and accessibility of water and services for the prevention and containment of fires including fires in wild lands.

Fire suppression will be provided for Blackstone Ranch Phase 1. This is accomplished by providing fire hydrants per Carson City standards and to the approval of the Carson City Fire and Engineering Departments. It is also important to note that new homes within the Lompa Ranch SPA are required to contribute \$1,000.00 per unit as a fire mitigation fee.



BLACKSTONE RANCH - PHASE 1

12. Recreation and trail easements.

Trail connections will be constructed within Blackstone Ranch Phase 1 per the requirements of the Lompa Ranch SPA. All necessary public use easements, etc. will be dedicated with final map.

Nevada Revised Statutes

Per item 34 of the tentative subdivision map application, the provisions NRS 278.349(3) are addressed in this section. Like the tentative map findings, NRS considerations are addressed in **bold face** type. Some NRS considerations are repetitive to Carson City adopted findings but are included to ensure complete compliance.

The governing body, or planning commission if it is authorized to take final action on a tentative map, shall consider:

(a) Environmental and health laws and regulations concerning water and air pollution, the disposal of solid waste, facilities to supply water, community or public sewage disposal and, where applicable, individual systems for sewage disposal;

Blackstone Ranch Phase 1 will be connect to City services. Waste disposal will therefore be managed in the same manner as other residential developments in the City. By utilizing the existing zoning and overall density, impacts from the project will be consistent with the City's goals and expectations.

(b) The availability of water which meets applicable health standards and is sufficient in quantity for the reasonably foreseeable needs of the subdivision;

The area is served by municipal utilities, including water. Additionally, fire hydrants will be provided per City standards. Water rights will be secured to serve the project, to the satisfaction of Carson City Engineering Department.

(c) The availability and accessibility of utilities;

The site is bordered by municipal utilities. They are therefore both available and accessible.



BLACKSTONE RANCH - PHASE 1

(d) The availability and accessibility of public services such as schools, police protection, transportation, recreation and parks;

The site is served by existing roads and is within walking distance of Mills Park and Carson High. Public services already extend to development that adjoins the site. In effect, the site is fully served by City services. It is also important to note that new public facilities, including a 10 acre community park will be included within the Lompa Ranch SPA as it continues to build-out.

(e) Conformity with the zoning ordinances and master plan, except that if any existing zoning ordinance is inconsistent with the master plan, the zoning ordinance takes precedence;

The current SF-6 zoning is consistent with the current Master Plan designation. This zoning is to remain in place with this project. The overall density of the site and the proposed structures are consistent with the zoning regulations.

(f) General conformity with the governing body's master plan of streets and highways;

The project conforms to the Master Plan for streets in that it locates development along an existing street. No changes to streets or highways are required.

(g) The effect of the proposed subdivision on existing public streets and the need for new streets or highways to serve the subdivision;

As noted above, no new streets or highways are required. A comprehensive traffic impact analysis is attached to this report.

(h) Physical characteristics of the land such as floodplain, slope and soil;

Engineering improvements, including new drainage improvements, will be implemented to ensure that all new units are not located within areas prone to flooding. There are no slope or soil constraints that would preclude development at the densities being proposed.



BLACKSTONE RANCH - PHASE 1

(i) The recommendations and comments of those entities and persons reviewing the tentative map pursuant to NRS 278.330 to 278.3485, inclusive;

Comments received on this application will be reviewed and discussed as needed. Any required amendments to the project will be incorporated or resolved to the satisfaction of Carson City staff.

(j) The availability and accessibility of fire protection, including, but not limited to, the availability and accessibility of water and services for the prevention and containment of fires, including fires in wild lands; and

The project site is not located within a wildland fire zone. Fire hydrants will be provided per City standards and the project will ultimately contribute \$189,000 in fire mitigation fees.

(k) The submission by the subdivider of an affidavit stating that the subdivider will make provision for payment of the tax imposed by chapter 375 of NRS and for compliance with the disclosure and recording requirements of subsection 5 of NRS 598.0923, if applicable, by the subdivider or any successor in interest.

A tax certificate for the parcel included within this application is included as an attachment to this report.

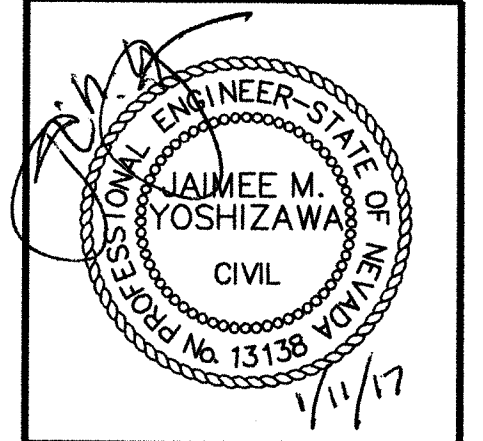
TENTATIVE MAP FOR BLACKSTONE RANCH - PHASE 1 CARSON CITY, NEVADA



7272 S. El Capitan Way
Suite 2
Las Vegas, NV 89148

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www.theredtd.com

R | REAL ESTATE
E | ENGINEERING
D | DEVELOPMENT



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BLACKSTONE DEVELOPMENT GROUP
439 PLUMB LANE
RENO, NV 89509
CONTACT: JOSHUA MYERS
PHONE: (775) 352-4200

OWNER

THOMAS B. KEATING AND MARTH L. KEATING TRUSTEES OF THE TOM AND MARTHA KEATING FAMILY TRUST DATED APRIL 5, 1995.
161 PLANTATION DR
CARSON CITY, NV 89703

DEVELOPER

BLACKSTONE DEVELOPMENT GROUP
439 PLUMB LANE
RENO, NV 89509
CONTACT: JOSHUA MYERS
PHONE: (775) 352-4200
FAX: N/A

LANDSCAPE ARCHITECT

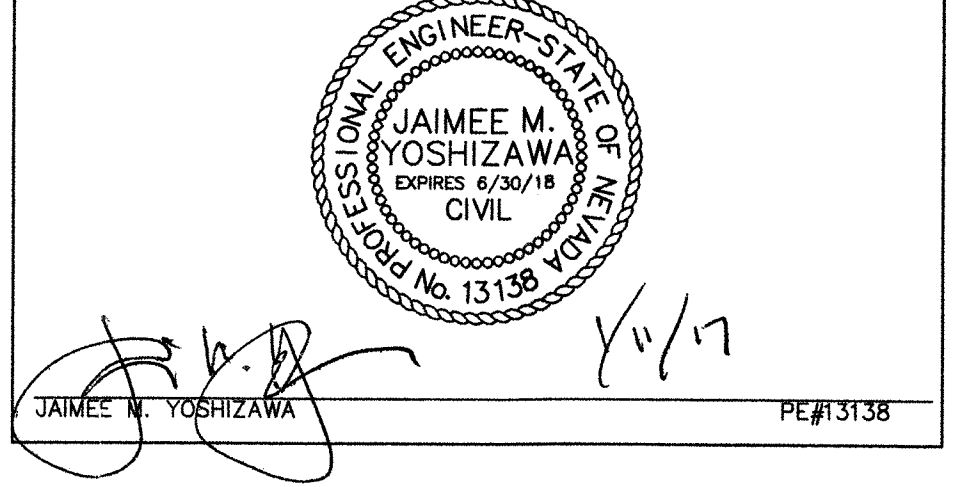
JOHN JONES LANDSCAPE ARCHITECT
8276 SOUTH EASTERN AVENUE, SUITE 200
LAS VEGAS, NEVADA 89123
CONTACT: JOHN JONES
JOHN@JOHNJONESLANDSCAPE.COM
PHONE: (702) 403-4320
FAX: N/A

CIVIL ENGINEER

THE RED LTD
7272 S. EL CAPITAN WAY, #2
LAS VEGAS, NEVADA 89148
CONTACT: JAIMEE M. YOSHIZAWA
JAIMEE@THEREDTD.COM
PHONE: (702) 728-5804
FAX: (702) 946-0865

ENGINEER'S STATEMENT

I, JAIMEE M. YOSHIZAWA, DO HEREBY CERTIFY THAT THIS MAP HAS BEEN COMPLETED BY ME, OR UNDER MY SUPERVISION AND WAS COMPLETED ON THIS 31ST DAY OF OCTOBER, 2016.



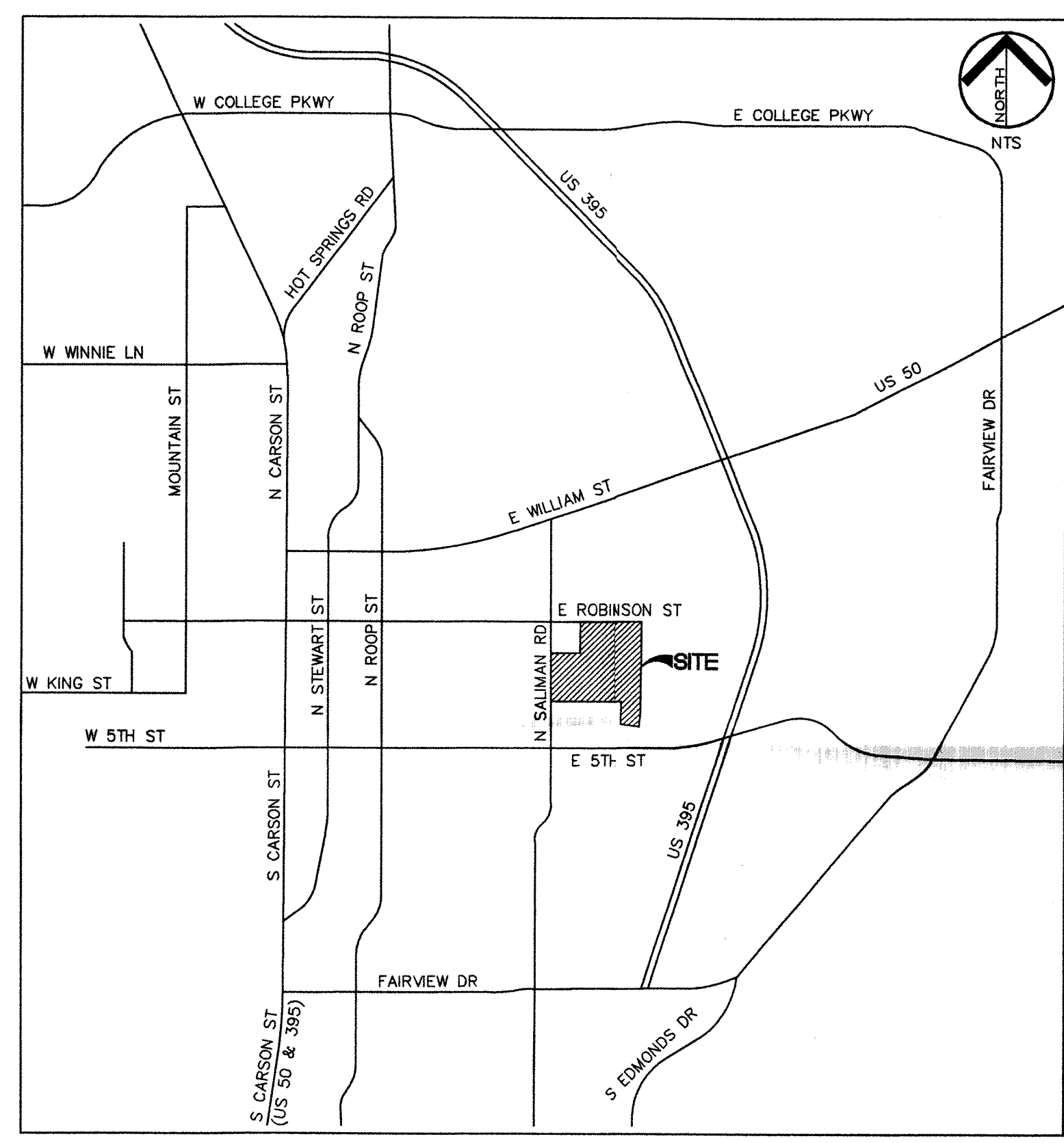
ABBREVIATIONS

AC	ASPHALTIC CONCRETE	INV	INVERT ELEVATION
BC	BACK OF CURB	LS	LANDSCAPE
BCR	BEGIN CURB RETURN	MH	MANHOLE
BM	BENCHMARK	MUE	MUNICIPAL UTILITY EASEMENT
BSW	BACK OF SIDEWALK	NTS	NOT TO SCALE
BVC	BEGIN VERTICAL CURVE	OH	OVERHEAD POWER
CL OR CL	CENTERLINE	PC	POINT OF CURVATURE
CLSM	CONTROLLED LOW STRENGTH MATERIAL	PE	PAID ELEVATION
CMAP	CORRUGATED METAL ARC PIPE	PIV	POST INDICATOR VALVE
CO	SANITARY SEWER CLEANOUT	PL	PROPERTY LINE
CON	CONCRETE	PP	POWER POLE
CY	CUBIC YARD	PRC	POINT OF REVERSE CURVE
DCDA	DOUBLE CHECK DETECTOR ASSEMBLY	PRV	PRESSURE REDUCING VALVE
DCSWCS	DESIGN AND CONSTRUCTION STANDARDS FOR WASTEWATER COLLECTION SYSTEMS	PT	POINT OF TANGENCY
EC	EDGE OF CONCRETE	PVC	POLY VINYL CHLORIDE
ECR	END CURB RETURN	PVI	POINT OF VERTICAL INTERSECTION
EGL	ENERGY GRADE LINE	RCB	REINFORCED CONCRETE BOX
EOP	EDGE OF PAVEMENT	RCP	REINFORCED CONCRETE PIPE
EVC	END VERTICAL CURVE	RPPA	REDUCED PRESSURE PRINCIPLE ASSEMBLY
EX	EXISTING	S	SLOPE
FC	FACE OF CURB	SD	STORM DRAIN LINE
FDC	FIRE DEPARTMENT CONNECTION	SDR	STANDARD DIMENSION RATIO
FF	FINISHED FLOOR	SF	SQUARE FEET
FG	FINISH GRADE	SL	STREET LIGHT
FH	FIRE HYDRANT	SS	SANITARY SEWERLINE
FL	FLOWLINE	SSMH	SANITARY SEWER MANHOLE
GB	GRADE BREAK	STA	STATION
HGL	HYDRAULIC GRADE LINE	SW	SIDEWALK
HP/LP	HIGH POINT /LOW POINT	SY	SQUARE YARD
		TC	TOP OF CURB
		TOE	TOE OF SLOPE
		TRW	TOP RETAINING WALL
		TYP	TYPICAL
		VG	VALLEY GUTTER
		WM	WATER METER

SHEET INDEX

PAGE NO	SHEET NO	SHEET NAME
1	C0	TITLE SHEET
2	C1	GENERAL NOTES
3	CS0	OVERALL SITE PLAN
4	CS1	SITE PLAN (1 OF 6)
5	CS2	SITE PLAN (2 OF 6)
6	CS3	SITE PLAN (3 OF 6)
7	CS4	SITE PLAN (4 OF 6)
8	CS5	SITE PLAN (5 OF 6)
9	CS6	SITE PLAN (6 OF 6)
10	CG1	GRADING PLAN (1 OF 6)
11	CG2	GRADING PLAN (2 OF 6)
12	CG3	GRADING PLAN (3 OF 6)
13	CG4	GRADING PLAN (4 OF 6)
14	CG5	GRADING PLAN (5 OF 6)
15	CG6	GRADING PLAN (6 OF 6)
16	CX0	CROSS SECTIONS
17	CU0	OVERALL UTILITY PLAN
18	CU1	UTILITY PLAN (1 OF 6)
19	CU2	UTILITY PLAN (2 OF 6)
20	CU3	UTILITY PLAN (3 OF 6)
21	CU4	UTILITY PLAN (4 OF 6)
22	CU5	UTILITY PLAN (5 OF 6)
23	CU6	UTILITY PLAN (6 OF 6)
24	CEO	EROSION CONTROL PLAN
25	CF0	FLOODZONE DESIGNATION
26	L-1	CONCEPTUAL LANDSCAPE PLAN
27	L-2	CONCEPTUAL LANDSCAPE PLAN
28	L-3	CONCEPTUAL LANDSCAPE PLAN
29	L-4	CONCEPTUAL LANDSCAPE PLAN
30	L-5	CONCEPTUAL LANDSCAPE PLAN
31	L-6	CONCEPTUAL LANDSCAPE PLAN

VICINITY MAP NTS



BASIS OF BEARINGS

NEVADA STATE PLANE COORDINATE SYSTEM, WEST ZONE, NORTH AMERICAN DATUM OF 1983/1994 (NAD 83/94). BEARINGS AND DISTANCES HEREON REFLECT GRID COORDINATES MULTIPLIED BY A COMBINED GRID TO GROUND FACTOR OF 1.0002 (ALSO KNOWN AS CARSON CITY MODIFIED).

BASIS OF ELEVATION

ELEVATIONS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAV88), AS ESTABLISHED BY USGS BENCHMARK V357, PID KR0998.

PROJECT LOCATION

A PORTION OF SECTION 16, TOWNSHIP 15 NORTH, RANGE 20 EAST, M.D.M.

PROJECT DATA

ASSESSOR PARCEL NUMBER(S)	: A PORTION OF 10-041-70 (44.55 ACRES)
LAND USE DESIGNATION	: MEDIUM DENSITY RESIDENTIAL
CURRENT ZONING	: A (AGRICULTURAL)
PROPOSED/APPROVED ZONING	: SF6 (SINGLE-FAMILY 6,000 SF), ZONING SUBJECT TO RECORDATION OF DEED
LOT DATA	
TOTAL LOTS AND SIZE	: 189 TOTAL LOTS (94 LOTS, 6,000 SF MIN. 91 LOTS, 7,000 SF MIN.)
ALLOWABLE DENSITY	: 3-8 DU/ACRE
PROPOSED DENSITY	: 4.25 DU/ACRE
SUMMATION OF GROSS AND NET LAND AREA	
TOTAL GROSS AREA	: 62.0 ACRES (PROPOSED SITE - 44.5 ACRES NOT A PART - 17.5 ACRES)
"PROPOSED SITE" GROSS AREA	: 44.5 ACRES
"NOT A PART" GROSS AREA	: 17.5 ACRES
TOTAL LOT AREA	: 31.1 ACRES
PROPOSED RIGHT-OF-WAY	: 10.8 ACRES
OPEN SPACE REQUIRED	: 0.7 ACRES 28,350 SF (150 SF OPEN SPACE/DU)
OPEN SPACE PROPOSED	: 2.6 ACRES 115,137 SF (893 SF OPEN SPACE/DU)
FLOOD ZONE DESIGNATION	
FEMA FLOOD HAZARD ZONE (EXISTING AND PROPOSED)	: REFERENCE SHEET CF0 (24 OF 30). *EXISTING AND PROPOSED FLOOD ZONE DESIGNATIONS*

APPROVED	REVISIONS	NO.	DATE

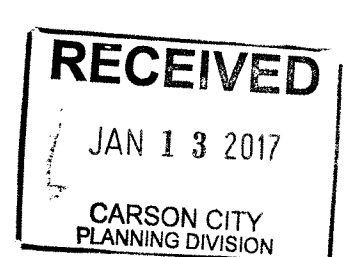
TITLE SHEET

BLACKSTONE RANCH - PHASE 1

CARSON CITY, NEVADA

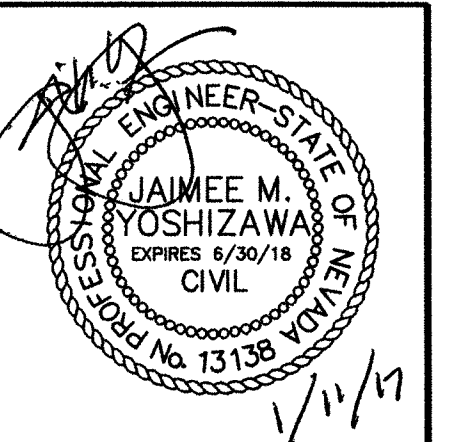
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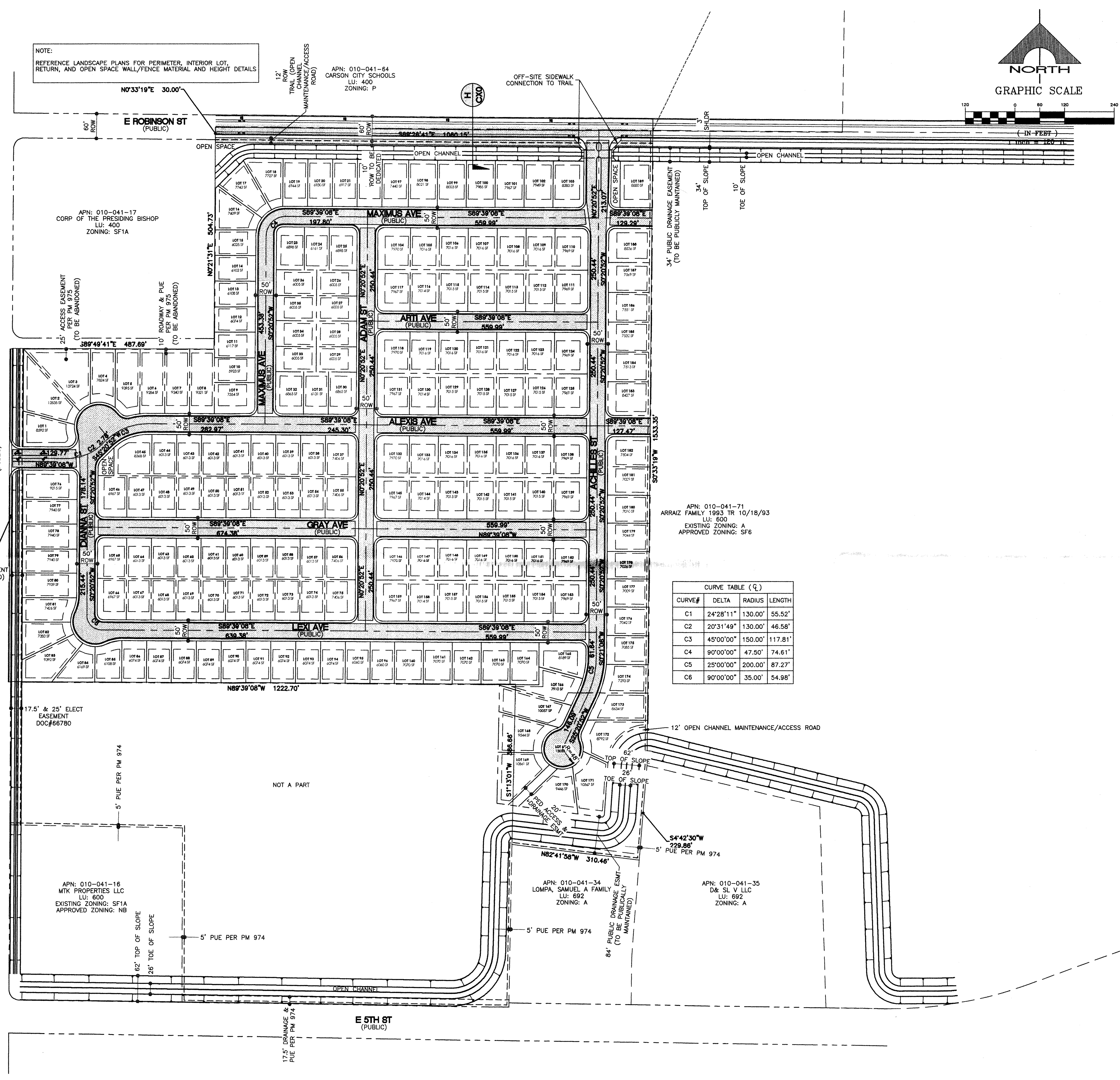


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 CONTACT: JOSHUA MYERS
 PHONE: (775) 352-4200

DATE	NO.	REVISIONS	APPROVED

OVERALL SITE PLAN
 BLACKSTONE RANCH - PHASE 1
 CARSON CITY, NEVADA

PG. JOB NO.	ENR8-000
DESIGNED BY	MAS
DRAWN BY	MAS
CHECKED BY	JY
DATE	2017 01 16



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